



# **Regina Planning Commission**

**Wednesday, January 8, 2020  
4:00 PM**

**Henry Baker Hall, Main Floor, City Hall**



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**OFFICE OF THE CITY CLERK**

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**Public Agenda  
Regina Planning Commission  
Wednesday, January 8, 2020**

**Approval of Public Agenda****Adoption of Minutes**

Regina Planning Commission - Public - Nov 6, 2019 4:00 PM

**Administration Reports**

RPC20-1      Discretionary Use Application (PL201900029) Proposed Child Daycare Centre - 1300 N Courtney Street

**Recommendation**

The Regina Planning Commission recommends that City Council:

1. Approve the discretionary use application for a proposed Child Day Care Centre located on proposed Parcel 130 at 1300 N Courtney Street in the Rosewood Park Concept Plan.
2. Direct Administration to issue a development permit subject to the following conditions:
  - a. The development shall be generally consistent with the plans attached to this report as Appendix A-3.1 to A-3.4 inclusive, prepared by NewRock Developments and dated November 15, 2019.
  - b. The development is subject to the execution of a service agreement and subdivision for Rosewood Park Phase 1, Stage 2.
  - c. The development shall be subject to Ministry of Education approval.
  - d. The development shall comply with all applicable standards and regulations in *Regina Zoning Bylaw No. 9250*.
3. Approve these recommendations at its January 29, 2020 meeting.

RPC20-2      Street Closure Application 19-CL-02



## OFFICE OF THE CITY CLERK

### **Recommendation**

Regina Planning Commission recommends that City Council:

1. Approve the application to close the portion of Victoria Avenue Service Road N. as shown in Appendix A-1.
2. Designate Blk/Par G-Plan FT2014 Ext 3 located at 140 Coleman Crescent as Municipal Reserve.
3. Direct the City Solicitor to prepare the necessary Bylaw.
4. Approve these recommendations at its January 29, 2020 meeting, to allow sufficient time for advertising the required public notice for the respective bylaw.

RPC20-3      15-OCP-03 Proposed Yards Neighbourhood Plan

### **Recommendation**

Regina Planning Commission recommends that City Council:

1. Approve the application to amend *Design Regina: The Official Community Plan Bylaw No. 2013-48* by adding the Yards Neighbourhood Plan, attached as Appendix C, as Part B.18.
2. Direct the City Solicitor to prepare the necessary bylaw to amend *Design Regina: The Official Community Plan Bylaw No. 2013-48* by adding the Yards Neighbourhood Plan, attached as Appendix C, as Part B.18.
3. Approve these recommendations at its January 29, 2020 meeting, to allow sufficient time to advertise the required public notice for the respective bylaw.

## City Clerk's Report

RPC20-4      Review of Outstanding Items

### **Recommendation**

It is recommended that Regina Planning Commission:

1. Delete the following items from its List of Outstanding Items:

<b><u>Item</u></b>	<b><u>Committee</u></b>	<b><u>Subject</u></b>
RPC04-16	Regina Planning	Regina's Old Warehouse Business



## OFFICE OF THE CITY CLERK

	Commission	Improvement District: Warehouse District Planning Study
MN11-10	Regina Planning Commission	Zoning Bylaw - Contractor Yards in Residential Areas
CR18-116	Regina Planning Commission	Civic Naming Committee Guideline Review
RPC12-17	Regina Planning Commission	Rezoning and Discretionary use Application (12-Z-20/12-DU-24) - Proposed Fourplex - 4000 3rd Avenue, Windsor Place Subdivision
CR14-137	Regina Planning Commission	Lease of Road Right-of-Way

2. Forward the updated List of Outstanding Items to Executive Committee for information.

### Adjournment



AT REGINA, SASKATCHEWAN, WEDNESDAY, NOVEMBER 6, 2019

AT A MEETING OF REGINA PLANNING COMMISSION  
HELD IN PUBLIC SESSION

AT 4:00 PM

**These are considered a draft rendering of the official minutes. Official minutes can be obtained through the Office of the City Clerk once approved.**

Present: Councillor Barbara Young, in the Chair  
Councillor Jerry Flegel  
Councillor Bob Hawkins  
David Bale  
Frank Bojkovsky  
Biplob Das  
Andre Kroeger  
Jacob Sinclair  
Steve Tunison

Regrets: Adrienne Hagen Lyster  
Celeste York

Also in Attendance: Council Officer, Elaine Gohlke  
Legal Counsel, Cheryl Willoughby  
Executive Director, City Planning & Community Development, Diana Hawryluk  
Manager, Real Estate, Keith Krawczyk  
Manager, West Planning, Yves Richard  
Historical Information & Preservation Supervisor, Dana Turgeon  
Senior Engineer, Max Zasada

APPROVAL OF PUBLIC AGENDA

**Steve Tunison moved, AND IT WAS RESOLVED, that the agenda for this meeting be approved, as submitted, and that the delegations be heard in the order they are called forward by the Chairperson.**

ADOPTION OF MINUTES

**Frank Bojkovsky moved, AND IT WAS RESOLVED, that the minutes for the meeting held on October 9, 2019 be adopted, as circulated.**

### ADMINISTRATION REPORTS

Reports RPC19-35 Lane Closure Application (19-CL-01) Road Right-of-Way Adjacent to 2150 and RPC19-36 Zoning Bylaw Amendment Application (19-Z-09) - 2109 York Street were considered together:

RPC19-35 Lane Closure Application (19-CL-01) Road Right-of-Way Adjacent to 2150 Alexandra Street

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#### **Recommendation**

1. That the application for the closure of Road Right-of-Way adjacent to 2150 Alexandra Street, as shown on the attached plan of proposed subdivision prepared by Scott L. Colvin, dated June 26, 2019 and legally described as Lane in Block 40, Registered Plan No. I5211, be approved.
2. That the City Solicitor be directed to prepare the necessary bylaw.
3. That this report be forwarded to the November 25, 2019 meeting of City Council for approval, which will allow sufficient time for advertising of the required public notice for the respective bylaw.

RPC19-36 Zoning Bylaw Amendment Application (19-Z-09) - 2109 York Street

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#### **Recommendation**

1. That the application to rezone a portion of 2109 York Street, being Part of Lot 16, Block 40, Plan No. I5211 in CPR Annex subdivision, from PS – Public Service Zone to R1A – Residential Older Neighbourhood Detached Zone, be approved.
2. That the application to sell park lands, being a portion of Lot 16, Block 40, Plan I5211, be approved.
3. That the City Solicitor be directed to prepare the necessary bylaw to authorize the respective Zoning Bylaw amendment.
4. That this report be forwarded to the November 25, 2019 meeting of City Council for approval, which will allow sufficient time for advertising of the required public notices for the respective bylaw.

The following addressed the Commission:

- Linda Flaman, regarding RPC19-35 Lane Closure Application (19-CL-01) Road Right-of-Way Adjacent to 2150 Alexandra Street; and
- Brandi Muskaluk regarding RPC19-35 Lane Closure Application (19-CL-01) Road Right-of-Way Adjacent to 2150 Alexandra Street and RPC19-36 Zoning Bylaw Amendment Application (19-Z-09) - 2109 York Street.

**Biplob Das moved, that:**

- 1. The recommendation contained in report RPC19-35 Lane Closure Application (19-CL-01) Road Right-of-Way Adjacent to 2150 Alexandra Street, be concurred in.**

**The motion was put and declared CARRIED.**

- 2. The recommendation contained in report RPC19-36 Zoning Bylaw Amendment Application (19-Z-09) - 2109 York Street, be concurred in.**

**The motion was put and declared CARRIED.**

### TABLED REPORT

#### RPC19-27 Civic Naming Committee Guideline and Street Name List Review

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##### Recommendation

1. That the names in Appendix A be stricken from the “Street Where You Live” list;
2. That the name “Firefighters” be approved for use as a park name honouring all firefighters in Appendix B, with future firefighters’ awarded with the Fire Services Exemplary Medal to be added to future signage in a future Firefighters Park;
3. That a review of naming civic buildings and other features be explored after a Sponsorship Naming Policy has been approved;
4. That the Administration be directed to report back on a process for Mayor and Council Awards with assistance from the Civic Naming Committee (CNC) as needed;
5. That the CNC Guideline be amended to remove temporary and supplemental street naming for reasons of health and safety;
6. That Administration report back to City Council on the status of the CNC Guideline in the 2020 annual report, with a full review of the CNC Guideline to be brought to City Council in 2021; and
7. That the name “Riel”, which is in Appendix A, be commemorated in an alternative way that honours the legacy of Louis Riel and his importance to Regina, Saskatchewan and Canada, while preserving health and safety by recommending that the question be referred to Reconciliation Regina.

**Councillor Flegel moved, AND IT WAS RESOLVED, that the recommendation contained in the report be concurred in.**

ADMINISTRATION COMMUNICATION

RPC19-37 City of Regina Zoning Bylaw 2019-19 Presentation

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**Recommendation**

That this communication be received and filed.

**Councillor Hawkins moved, AND IT WAS RESOLVED, that this report be received and filed.**

ADJOURNMENT

**Councillor Hawkins moved, AND IT WAS RESOLVED, that the meeting adjourn.**

The meeting adjourned at 5:17 p.m.

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Chairperson

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Secretary

## Discretionary Use Application (PL201900029) Proposed Child Daycare Centre - 1300 N Courtney Street

<b>Date</b>	January 8, 2020
<b>To</b>	Regina Planning Commission
<b>From</b>	City Planning & Community Development
<b>Service Area</b>	Planning & Development Services
<b>Item No.</b>	RPC20-1

### RECOMMENDATION

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The Regina Planning Commission recommends that City Council:

1. Approve the discretionary use application for a proposed Child Day Care Centre located on proposed Parcel 130 at 1300 N Courtney Street in the Rosewood Park Concept Plan.
2. Direct Administration to issue a development permit subject to the following conditions:
  - a. The development shall be generally consistent with the plans attached to this report as Appendix A-3.1 to A-3.4 inclusive, prepared by NewRock Developments and dated November 15, 2019.
  - b. The development is subject to the execution of a service agreement and subdivision for Rosewood Park Phase 1, Stage 2.
  - c. The development shall be subject to Ministry of Education approval.
  - d. The development shall comply with all applicable standards and regulations in *Regina Zoning Bylaw No. 9250*.
3. Approve these recommendations at its January 29, 2020 meeting.

### ISSUE

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NewRock Developments Inc. (the Applicant), operating on behalf of the current owners,

Canadian Midwest District of The Christian And Missionary Alliance, proposes to develop a 90-space Child Day Care Centre on proposed Parcel 130 within the Rosewood Park Concept Plan. The subject property was zoned R5 – Residential Medium Density under *Regina Zoning Bylaw No. 9250* (Zoning Bylaw) under which application was made, where a Child Day Care Centre is a discretionary use, and is located with an area subject to an approved neighbourhood plan (Coopertown Neighbourhood Plan) and concept plan (Rosewood Park Concept Plan).

Child Day Care Centre is regulated as discretionary use, within the R5 – Residential Medium Density zone, in the Zoning Bylaw in order to ensure that the development is compatible in a residential context from the perspective of off-site impacts, parking and traffic.

The proposal has been assessed and is deemed to comply with the Zoning Bylaw; *Design Regina: The Official Community Plan Bylaw No. 2013-48* (OCP); the Coopertown Neighbourhood Plan and the Rosewood Park Concept Plan.

## **IMPACTS**

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### **Financial Impacts**

Capital funding to provide municipal infrastructure that is required for subdivision and development in the concept plan area will be the sole responsibility of the developer. The municipal infrastructure that is built and funded by the developer will become the City of Regina's (City) responsibility to operate and maintain through future budgets.

City Council recently approved (CR18-119) a two-year tax exemption for the years 2019 - 2020 equal to 40 per cent of the taxes levied with respect to all properties in the city that are owned and operated by licensed non-profit day care centres. Should the applicant meet the conditions of the exemption, they will be eligible.

### **Environmental Impacts**

The subject property is located within the Low Sensitivity Aquifer Protection Overlay Zone; therefore, will be required to comply with the applicable protection performance standards, which are reviewed at building permit phase. The proposed land-use (Child Day Care Centre) is not categorized as an industrial development and no environmental impacts, which might warrant analysis and mitigation measures, are anticipated.

### **Accessibility Impact**

The proposed development will provide one accessible parking stall, which conforms to the minimum requirement for the site.

### **Policy/Strategic Impact**

The proposed development supports the following OCP goals/ policies:

- Section D11, Goal 1, Policy 13.5: Encourage the provincial government and the community to establish locally based attainable childcare facilities, which are essential to enabling parents to secure access to employment.
- Section D5, Goal 1, Policy 7.1.4: Require that new neighbourhoods are planned and developed to include the following: Opportunities for daily lifestyle needs, such as services, convenience shopping, and recreation.

## **OTHER OPTIONS**

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Alternative options would be to refer the application back to Administration or deny the application.

## **COMMUNICATIONS**

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The applicant and other interested parties will receive a copy of the report and notification of the meeting to appear as a delegation in addition to receiving a written notification of City Council's decision.

## **DISCUSSION**

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The Rosewood Park neighbourhood is in the process of being developed and currently consists of a joint-use school (Plainsview School and St. Nicholas School) and a religious institution (Rosewood Park Alliance Church). Lands to the north are undeveloped; however, are identified for residential development in accordance with the Rosewood Park Concept Plan. Lands to the east, on the opposing side of Courtney Street, consists of the existing Maple Ridge neighbourhood.

The proposed development will include the following features:

- A 788 square metre building as shown in Appendix A-3.1 to A-3.4.
- Twenty-five parking spaces, which will be used for parking and drop off. Access to parking will be provided from Mapleford Boulevard. All parking will be contained on-site.

According to the Zoning Bylaw, the Child Day Care Centre requires one parking space and nine drop-off spaces; therefore, proposed development exceeds minimum requirements. The development, as proposed, conforms with the Zoning Bylaw.

In addition to meeting the City's requirements, the proposed development must be in conformity with the applicable regulatory requirements of the Government of Saskatchewan (Province). Regarding childcare facilities, the Province regulates such matters as capacity, space requirements, etc.

Subdivision of the Subject Property and the required servicing agreement have not yet been completed but are required prior to the proposed development proceeding (see Recommendation # 2b of this Report). Issuance of development and other permits is conditional on subdivision approval.

This application was submitted to the City before the new *Regina Zoning Bylaw 2019-19* came into effect and, accordingly, will be reviewed under the regulations of the *Regina Zoning Bylaw 9250* (Zoning Bylaw).

Letters were mailed to immediate property owners and a sign is posted on the site. The Sherwood Estates / McCarthy Park and Twin Lakes Community Associations were contacted. Twin Lakes Community Association responded with comments concerning on-street parking and traffic. A more detailed accounting of the public notice comments and the Twin Lakes Community Association comments is provided in Appendix B.

This application is deemed to be in compliance with all applicable policies and regulations and procedures regarding communications and engagement.

## DECISION HISTORY

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City Council's approval is required pursuant to Part V of *The Planning and Development Act, 2007*.

Respectfully submitted,



Fred Searle, Director  
Planning & Development Services

Respectfully submitted,



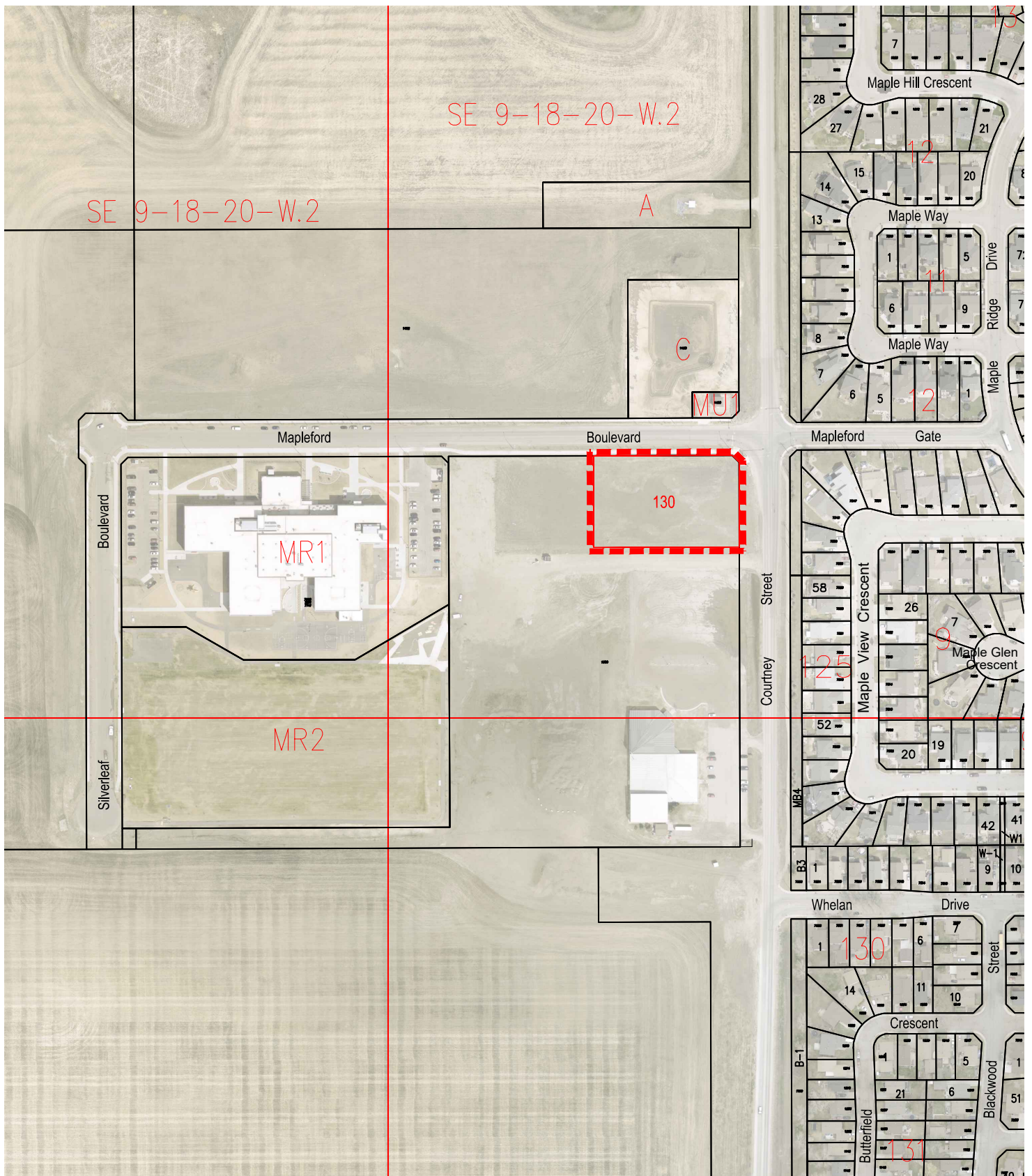
Diana Hawryluk, Executive Director  
City Planning & Community Development

Prepared by: Pam Ewanishin, City Planner II

## ATTACHMENTS

Appendix A-1  
Appendix A-2  
Appendix A-3.1  
Appendix A-3.1a  
Appendix A-3.2  
Appendix A-3.3  
Appendix A-3.4  
Appendix B



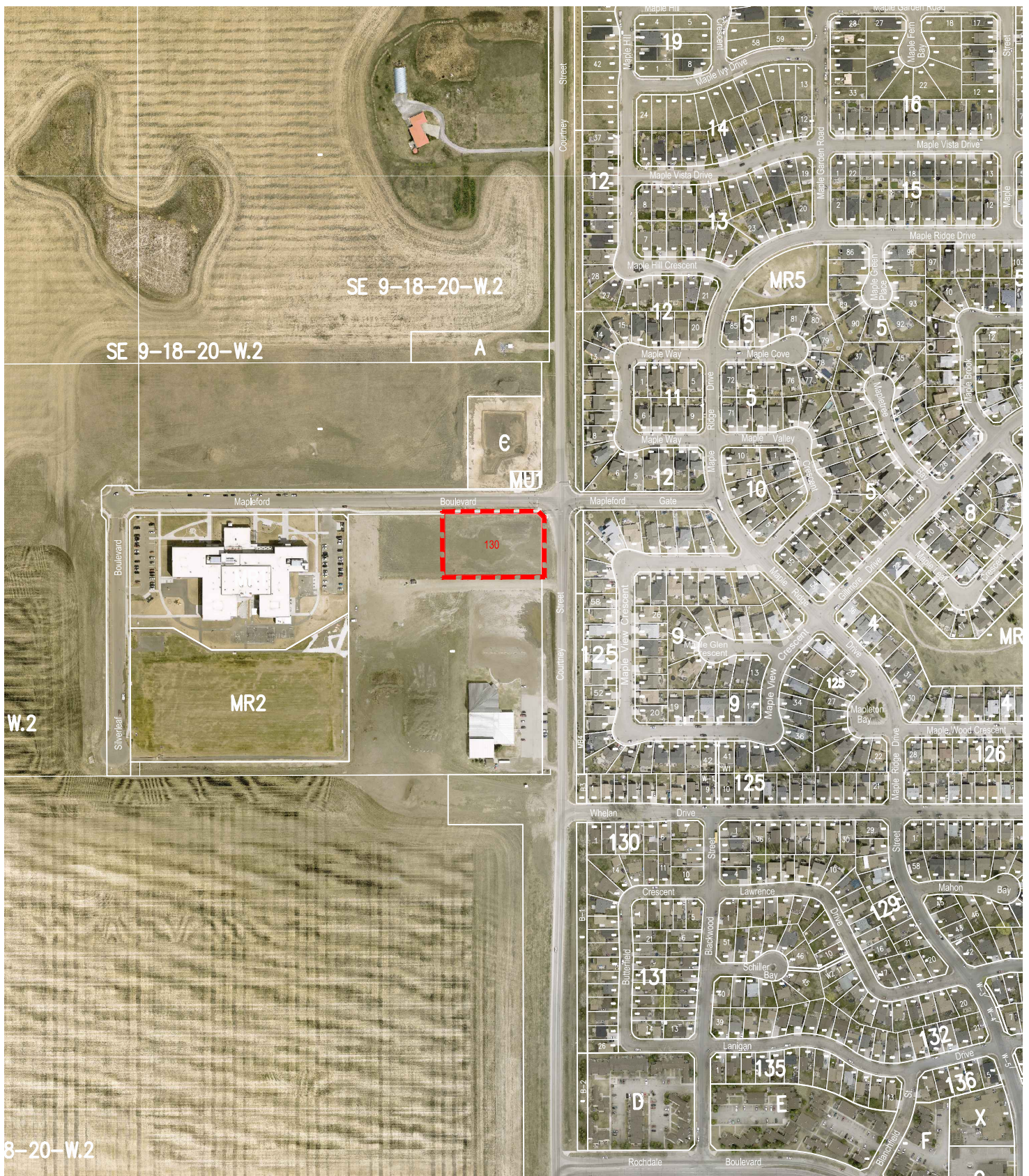


Subject Property

Date of Photography : 2018







Subject Property

Date of Photography: 2018



Project PL201900029

Civic Address/Subdivision

1300 N Courtney Street



MUNICIPAL ADDRESS: TBA  
LEGAL DESCRIPTION: LSD 1 SEC 09 TWP 18 RGE 20 W2 EXT 51

BLOCK 130		
R5 - MEDIUM DENSITY RESIDENTIAL ZONE		
DEVELOPMENT STANDARDS FOR DISCRETIONARY USE: DAYCARE		
STANDARD	REQUIREMENT	ACTUAL
MINIMUM LOT AREA	500 m <sup>2</sup> (0.12 ac)	5610.6 m <sup>2</sup> (1.4 ac)
MINIMUM LOT FRONTAGE	15.0 m	89.93 m
SITE COVERAGE	MAX. 50%	14%
FLOOR AREA RATIO	3.00	0.14

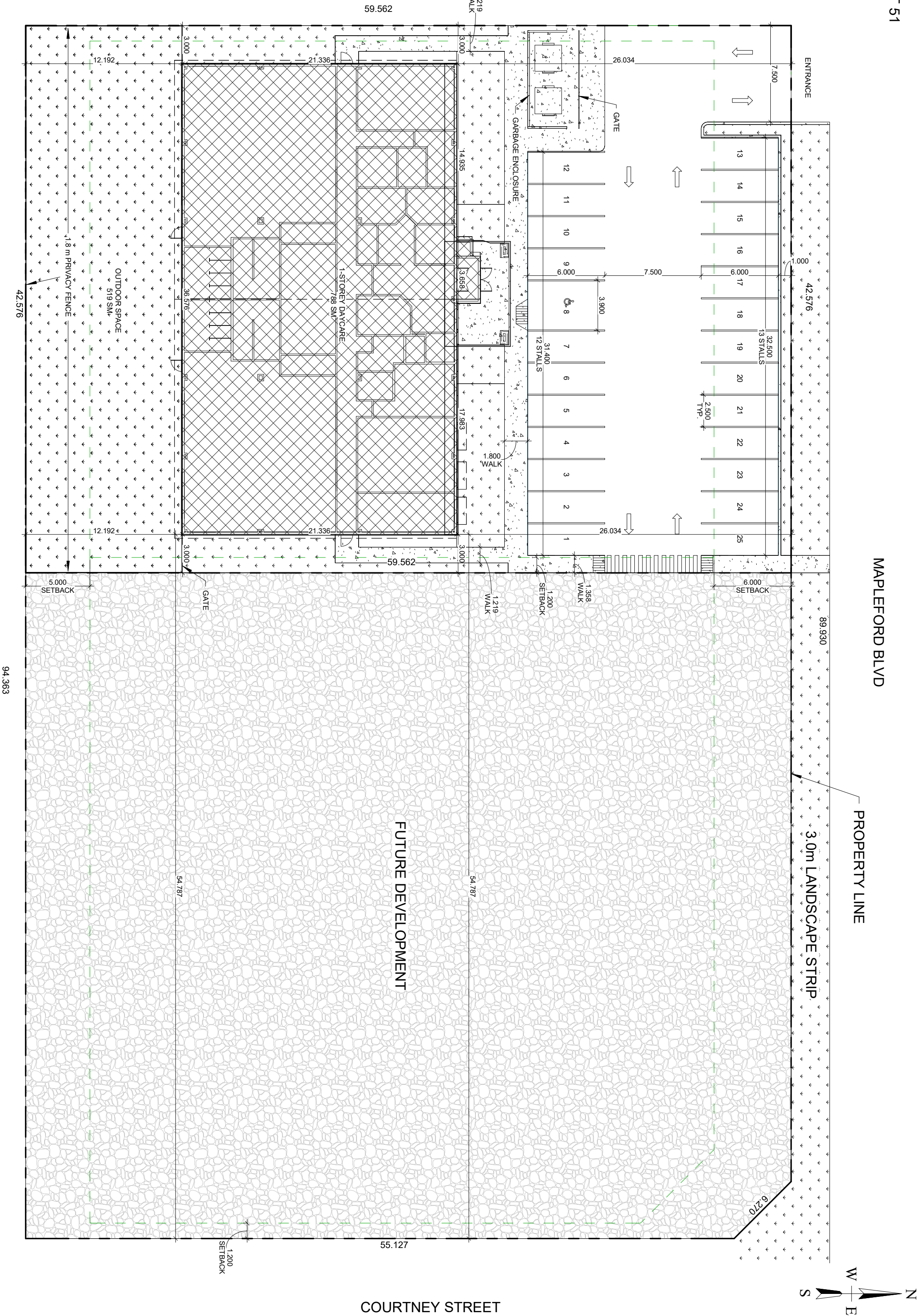
Topography Schedule		
Name	Area	Coverage
Asphalt	156.337 m <sup>2</sup>	13%
Grass	1769.877 m <sup>2</sup>	14%
Concrete	2119.624 m <sup>2</sup>	4%
Landscape	771.163 m <sup>2</sup>	14%
Undeveloped	3075.254 m <sup>2</sup>	50%
	5610.598 m <sup>2</sup>	100%

Parking Schedule	
Qty	Description
24	Parking Spaces 2.5 x 6.0 - 90°
1	Parking Space 3.9 x 6.0 - 90° Accessible
Grand total: 25	

NOTES

- ACCESS TO MAPLEFORD BOULEVARD MUST BE CONSTRUCTED AS PER THE CITY OF REGINA CONSTRUCTION STANDARD DRAWING R-10C
- SITE LIGHTING TO BE ACCOMMODATED BY WALL PACKS ON BUILDING (SEE BUILDING ELEVATIONS)

AREA TO BE DEVELOPED		
R5 - MEDIUM DENSITY RESIDENTIAL ZONE		
DEVELOPMENT STANDARDS FOR DISCRETIONARY USE: DAYCARE		
STANDARD	REQUIREMENT	ACTUAL
MINIMUM LOT AREA	500 m <sup>2</sup> (0.12 ac)	2535.6 m <sup>2</sup> (.63 ac)
MINIMUM LOT FRONTAGE	15.0 m	42.5 m
SITE COVERAGE	MAX. 50%	31%
FLOOR AREA RATIO	3.00	0.31
MAX. BUILDING HEIGHT	11.0 m	6.4 m
PARKING	5 SPACES PLUS 2 FOR EACH INCREMENT OF 15 BE ACCESSIBLE	TOTAL STALLS: 25 24 - 2.5m x 6.0m STALLS 1 - 4.8m x 6.0m ACCESSIBLE STALL = 9



1 Site Plan - Block 130  
1 : 250

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NEWROCK

DEVELOPMENTS INC.

671 Industrial Ave. SE, Medicine Hat, Alberta, T1A 3L5

Office: 403.529.1023 Fax: 403.529.1059

NewRock Developments Inc. reserves the right to make modifications to building and/or property design and modifications to specifications and/or features should they be necessary to maintain local building code requirements.

Revision Schedule		
No.	Description	Date
1	Permit Submission	2019-10-10
2	Application Changes	2019-11-01
3	Note Addition	2019-11-15

FOR		DRAWING TITLE	
Rosewood Park - Block 130		Site Plan	
PROJECT		CREATION DATE:	09/27/19
Daycare Design		BY:	SDR
Regina, SK		CHK:	--
		JOB NO.	Scale AS SHOWN



MUNICIPAL ADDRESS: TBA  
LEGAL DESCRIPTION: LSD 1 SEC 09 TWP 18 RGE 20 W2 EXT 51

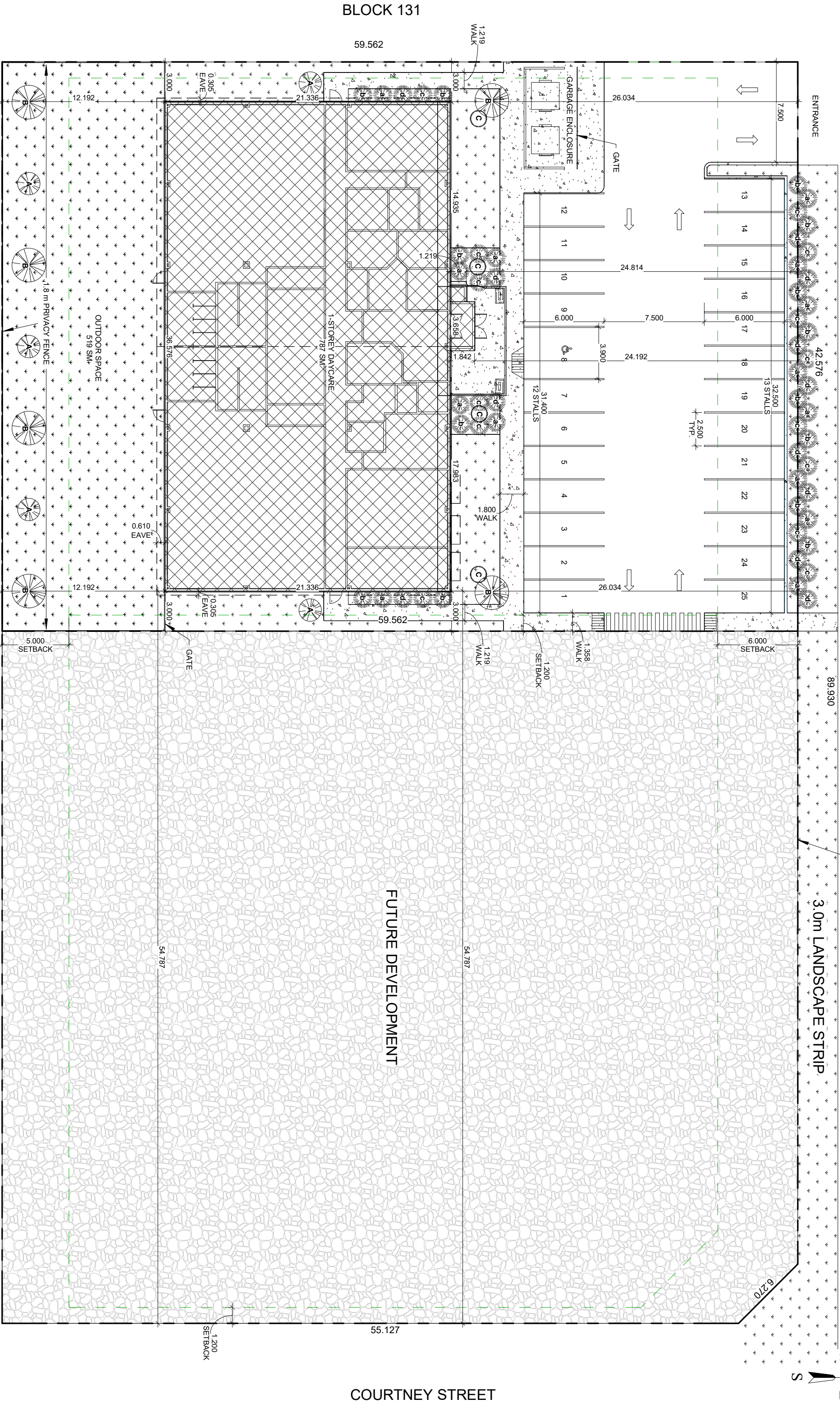
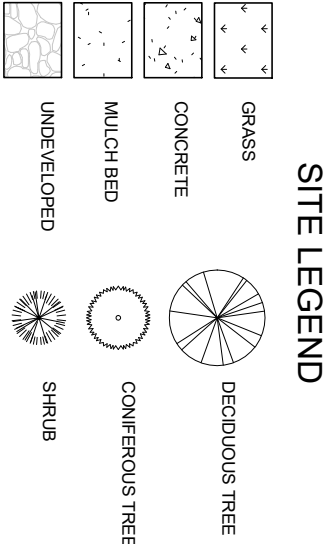
MAPLEFORD BLVD

PROPERTY LINE

3.0m LANDSCAPE STRIP

MIN. LANDSCAPING REQUIREMENTS		
DESCRIPTION	REGINAL LUB	ACTUAL
MIN. REQUIRED SITE LANDSCAPING	15%	31%
TREE QUANTITY	1 PER 40qm REQUIRED SITE LANDSCAPED AREA = 10	15
SHRUB QUANTITY	1 PER 20qm REQUIRED SITE LANDSCAPED AREA = 20	30

NOTES  
• SITE LIGHTING TO BE ACCOMMODATED BY WALL PACKS ON BUILDING (SEE BUILDING ELEVATIONS)



Landscaping Quantities	
Description	Area
Grass	724.947 m²
Mulch	36.816 m²
	771.763 m²

Topography Schedule	
Name	Area
Asphalt	725.537 m²
Building	728.607 m²
Concrete	219.624 m²
Landscape	771.763 m²
Undeveloped	3075.254 m²
	5610.568 m²
	100%

Planting Schedule			
Count	Type	Botanical Name	Common Name
	Shrub		
14	a	Cornus alba Sibirica	Siberian Coral Dogwood
14	b	Potentilla fruticosa 'Aboleswood'	Abbotswood Potentilla
14	c	Spiraea tomentosa	Three Lobed Spirea
12	d	Juniperus Scopulorum	Rocky Mountain Juniper
54			
	Tree		
5	A	Fraxinus pennsylvanica Prairie Spire	Prairie Spire Ash
6	B	Syringa Reticulata	Japanese Tree Lilac
4	C	Picea purgens	Colorado Spruce
15			
69			

1 Site - Block 130 Landscape Plan  
1 : 250

Count	Type	Botanical Name	Common Name	Size
	Shrub			
14	a	Cornus alba Sibirica	Siberian Coral Dogwood	450 mm Height Min.
14	b	Potentilla fruticosa 'Aboleswood'	Abbotswood Potentilla	450 mm Height Min.
14	c	Spiraea tomentosa	Three Lobed Spirea	450 mm Height Min.
12	d	Juniperus Scopulorum	Rocky Mountain Juniper	450 mm Height Min.
54				
	Tree			
5	A	Fraxinus pennsylvanica Prairie Spire	Prairie Spire Ash	40 mm Cal Min.
6	B	Syringa Reticulata	Japanese Tree Lilac	40 mm Cal Min.
4	C	Picea purgens	Colorado Spruce	1200 mm Height
15				
69				

NEWROCK

DEVELOPMENTS INC.

671 Industrial Ave., SE, Medicine Hat, Alberta, T1A 3L5  
Office: 403.529.1023 Fax: 403.529.1059

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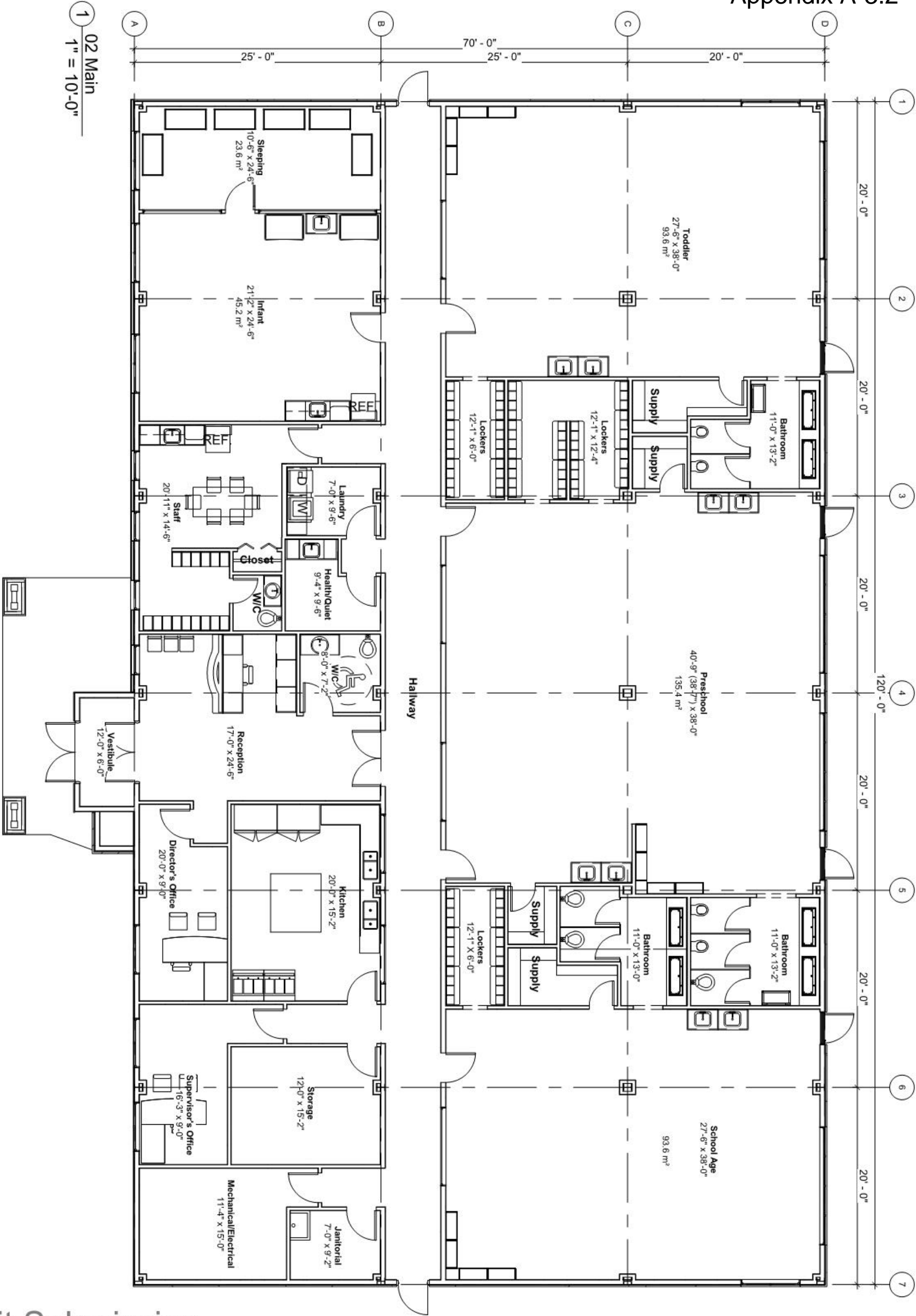
Drawings are not to be used for construction until reviewed and approved by the Authority Having Jurisdiction.

NewRock Developments Inc. reserves the right to make modifications to building and/or property design and modifications to specifications and/or features should they be necessary to maintain local building code requirements.

Revision Schedule		
No.	Description	Date
1	Permit Submission	2019-10-10
2	Application Changes	2019-11-01

FOR		DRAWING TITLE	
Rosewood Park - Block 130		Landscaping Plan	
PROJECT		CREATION DATE:	10/09/19
Daycare Design		BY:	SDR
Regina, SK		CHK:	--
		JOB NO.	Scale AS SHOWN





1 02 Main  
1" = 10'-0"

Permit Submission

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1	Permit Submission	2019-10-10
No.	Description	Date
Revision Schedule		

**NEWROCK**  
DEVELOPMENTS INC.

671 Industrial Ave. SE  
Medicine Hat, Alberta, T1A 3L5  
Office: 403.529.1023 Fax: 403.529.1059

FOR  
Rosewood Park Dev.

PROJECT  
Daycare

Regina, SK

DRAWING TITLE  
Main Floor Plan

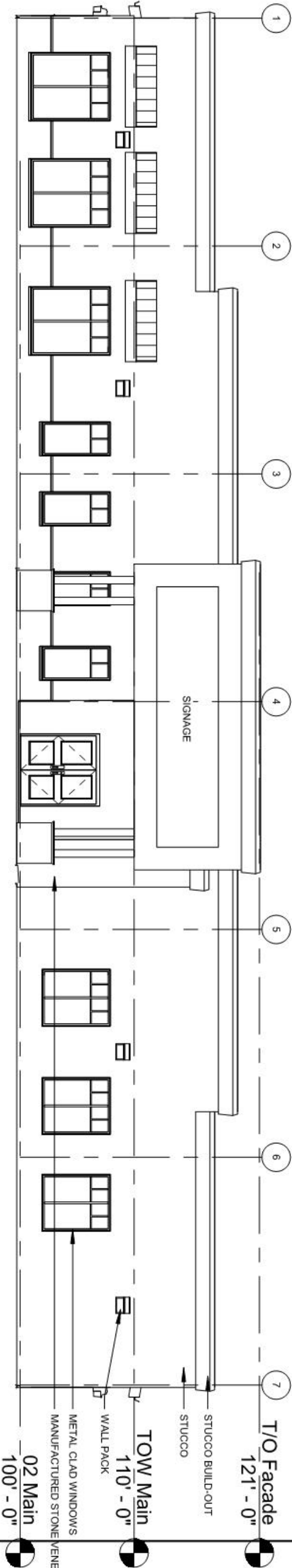
BY: SDR      CHK: --

SCALE: AS SHOWN

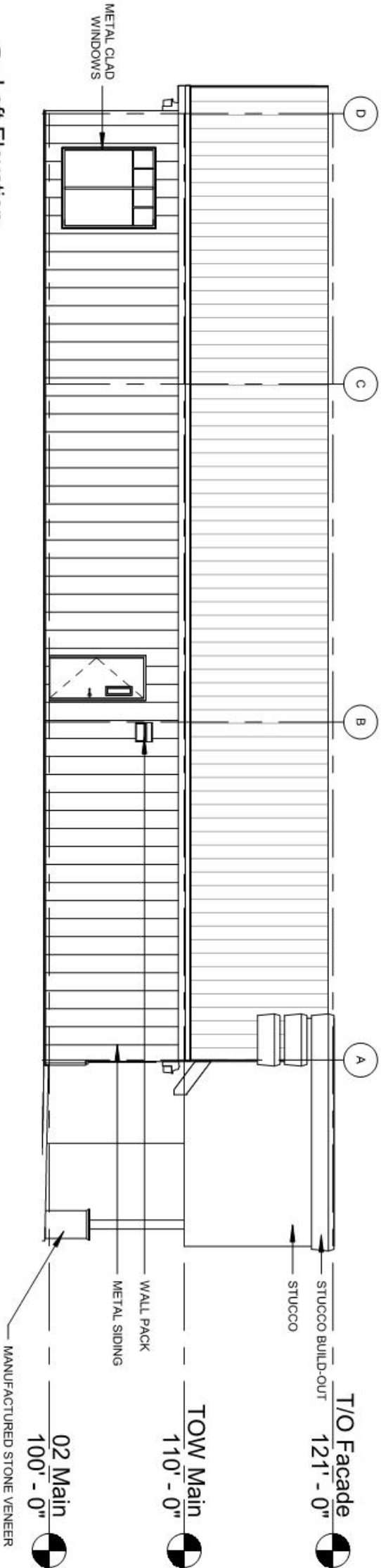
CREATION DATE: 04/23/19

DWG NO.

A110



1 Front Elevation  
1" = 10'-0"



2 Left Elevation  
1" = 10'-0"

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1	Permit Submission	2019-10-10
No.	Description	Date
Revision Schedule		

**NEWROCK**  
DEVELOPMENTS INC.

671 Industrial Ave. SE  
Medicine Hat, Alberta, T1A 3L5  
Office: 403.529.1023 Fax: 403.529.1059

FOR  
Rosewood Park Dev.

PROJECT  
Daycare

Regina, SK

DRAWING TITLE  
Elevations

BY: SDR

CHK: --

SCALE: AS SHOWN

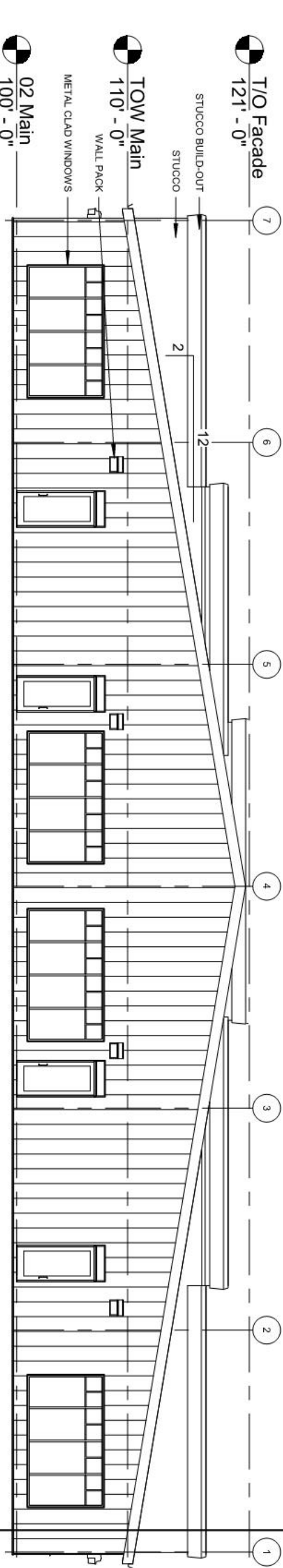
CREATION DATE: 09/17/19

DWG NO.

Permit Submission

A201



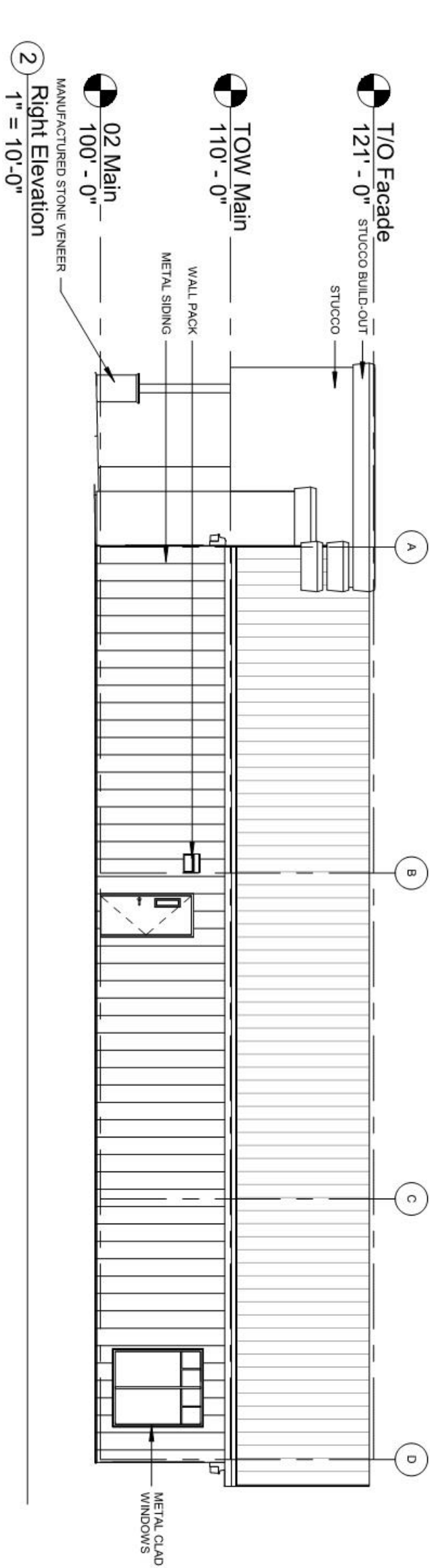


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1 Rear Elevation  
1" = 10'-0"



2 Right Elevation  
1" = 10'-0"

Permit Submission

1 Permit Submission 2019-10-10	
No.	Description Date
Revision Schedule	
<b>NEWROCK</b> DEVELOPMENTS INC.	
671 Industrial Ave. SE Medicine Hat, Alberta, T1A 3L5	
Office: 403.529.1023 Fax: 403.529.1059	
FOR Rosewood Park Dev.	
PROJECT Daycare	
Regina, SK	
DRAWING TITLE Elevations	
BY: SDR	CHK: --
SCALE: AS SHOWN	
CREATION DATE: 09/17/19	
DWG NO. A202	

Public Notice Comments

Response	Number of Responses	Issues Identified
<i>Completely opposed</i>		
<i>Accept if many features were different</i>		
<i>Accept if one or two features were different</i>	1	<ul style="list-style-type: none"> <li>- I approve of the location and the single-story plan.</li> <li>- Suggest the developer consider a “drive-thru/drop-off/pick-up zone”</li> <li>- Concerns of major traffic congestion during peak drop-off / pick up time.</li> <li>- Safety concerns with pedestrian traffic along Courtney going into Mapleford Blvd. The pedestrian sidewalk needs to be completed from Whelan Drive to Mapleford Gate.</li> <li>- This would be a welcome addition to the neighbourhood.</li> </ul>
<i>I support this proposal</i>		

Twins Lakes Community Association Comments

- There is no on-street parking during peak hours. There is concern that there will be additional traffic build up with 90 child daycare. There is already a lot of parking and traffic created from the schools.
- Which road will the daycare be accessed from? If access is off of Mapleford it will impact current parking availability for visitors to the schools as well as create potential issues for school buses navigating Mapleford Boulevard. Access too close to the traffic lights on either Mapleford or Courtney would also be of concern for both vehicle and pedestrian safety.
- The schools already have a daycare in them so the proximity of another large daycare on the block will definitely impact traffic and a site impact traffic study would be useful especially since the area is not currently developed at this time. We are not sure if the current configuration of the intersection will handle the increased traffic volume on full development of the area.
- Parents from St. Nicholas school are already accessing the field area from the Rosewood Park entrance to park in the field east of the schools for events and to pick up and drop off children at the school due to limited on street parking at the schools due to the volume of people requiring access to 2 schools and the Hopes Home daycare so access to the main roadways will be of major concern to Mapleford Gate and Courtney Street current users.
- Adequate snow clearing should be a higher priority if such a facility proceeds as there have already been issues with the current schools and proposed development to the North of the schools in the next year will also impact traffic in this area and buses to the school with having one route to take.



**1. Issue Consider a Drive-Thru for Drop-off / Pick-up**

*Administration's Response:*

The drop-off spaces that were provided on the plan are compliant with the regulations of the Regina Zoning Bylaw No. 9250.

*Applicant's Response:*

This was looked at early on, but in order to fit the building, required parking, outside play area, a drive-thru for drop-off and pick-up could not be accommodated. In order for a drive-thru to work there would have to be at least 2 lanes, so parents would not get stuck waiting after dropping-off children for other parents who may take long to drop-off children. If more land was added to accommodate this it would result in higher land cost, which could put the day care project in jeopardy due to the overall costs.

**2. Issue Sidewalk Along Courtney Street**

*Administration's Response:*

There is a pathway along the east side of the road which is considered as a multi-use pathway that extends from Whelan Drive to Maplefors Gate. The NW link pathway currently ends at Rink Avenue but is planned to be extended to Whelan Drive to tie it together.

**3. Issue What Street does the Subject Property Access From?**

*Administration's Response:*

The proposed development will have access from Maplefors Boulevard.

**4. Issue Traffic Concerns**

*Administration's Response:*

This application is a new development area, when road networks are designed in new neighborhoods, they are built with consideration to expected traffic trips generated based on Zoning and Density. This property has already been given a zoning designation, the traffic for this development on the internal road network has already been taken into consideration.

- Maplefors Blvd is classified as a collector roadway, collector roadways have the capacity to carry up to 12,000 vehicles per day. The daycare traffic will not bring the vehicles per day travelling along Maplefors Blvd to or over capacity.

**5. Issue Parking Concerns**

*Administration's Response:*

The proposed development has met the requirements within the Zoning Bylaw for number of parking and drop off spaces. The regulation for drop off spaces is based on number of children that are accommodated at the facility.

## Street Closure Application 19-CL-02

<b>Date</b>	January 8, 2020
<b>To</b>	Regina Planning Commission
<b>From</b>	City Planning & Community Development
<b>Service Area</b>	Planning & Development Services
<b>Item No.</b>	RPC20-2

### RECOMMENDATION

---

Regina Planning Commission recommends that City Council:

1. Approve the application to close the portion of Victoria Avenue Service Road N. as shown in Appendix A-1.
2. Designate Blk/Par G-Plan FT2014 Ext 3 located at 140 Coleman Crescent as Municipal Reserve.
3. Direct the City Solicitor to prepare the necessary Bylaw.
4. Approve these recommendations at its January 29, 2020 meeting, to allow sufficient time for advertising the required public notice for the respective bylaw.

### ISSUE

---

The application proposes to close a portion of the Victoria Avenue Service Road N., as depicted in Appendix A-1. The portion of street is no longer needed for use by the travelling public as the re-alignment of Eastgate Drive now replaces this former road.

### IMPACTS

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#### Policy / Strategic Impact

Section B, Financial Policies: The proposal is consistent with the policies in *Design Regina: The Official Community Plan Bylaw No. 2013-48* (OCP) with respect to the community goal of achieving long-term financial viability. By divesting itself of an unused portion of road right-of-way, the City of Regina (City) has ensured that there will not be any long-term

financial implications associated with the land.

### **Financial**

The street closure will relieve the City of any financial obligations for its maintenance or physical condition even though the City will be responsible for maintaining the recently dedicated new portion of Eastgate Drive.

There are no environmental, risk/legal, or other implications and considerations.

### **OTHER OPTIONS**

---

Administration recommends approval of the application to close the portion of the street as depicted in Appendix A-1.

An alternative option is to deny the application. Implication of denial is that the road right of way would not be legally closed which is not desirable as it is no longer functioning as a roadway.

### **COMMUNICATIONS**

---

The applicant and other interested parties will receive a copy of the report and notification of the meeting to appear as a delegation, in addition to receiving written notification of City Council's decision.

### **DISCUSSION**

---

City of Regina, Land and Real Estate Management Department, has applied to close a portion of the Victoria Avenue Service Road N., which connects to Eastgate Drive, as depicted on Appendix A-1. In 2015, the Victoria Avenue Service Road bridges over Pilot Butte Creek were replaced temporarily and in 2018 Eastgate Drive was re-aligned approximately 65 metres to the north. This now serves as the service road crossing over Pilot Butte Creek. The realignment of Eastgate Drive improved traffic safety at the intersection of Coleman Crescent and Eastgate Drive.

The City intends to continue to hold the property as road allowance that can be used as part of the boulevard, sidewalk and drainage from the Coleman Crescent, Eastgate Drive and Victoria Avenue. It will not be consolidated with adjacent parcels.

As a follow up to report CM18-8 dated May 28, 2018, through this recommendation, the land described as Blk/Par G-Plan FT2014 Ext 3 and located at 140 Coleman Crescent was identified to be dedicated as Municipal Reserve to replace an area of equal to or greater than the area being used to accommodate roadway realignment.

The subject property is currently zoned PS - Public Service and the surrounding land uses are Victoria Avenue to the south, open space land to the north, Coleman Crescent to the west and Eastgate Drive and commercial land uses to the east.

Administration recommends approving the application because:

1. The proposed development is consistent with the policies in the OCP.
2. The proposed stretch of the road is temporarily closed with the construction of a new road immediately north of this location in 2018. The street is no longer needed for use by the travelling public and is not functioning as a roadway.

Letters were mailed out to property owners in the vicinity, and statutory notice will be published in the *Regina Leader-Post*. The Dewdney East Community Association was advised of the application. Administration received one comment from the public and there were no issues raised through this outreach. A summary of feedback is provided in Appendix B.

## DECISION HISTORY

---

On June 20, 2016 the Public Works Committee approved in principle the new Eastgate Drive alignment and creek crossing to Coleman Crescent (PW116-8).

In May of 2018, City Council considered report CM18-8 and approved the use of Public Reserve space for the Eastgate Drive realignment. The recommendation also required Administration to dedicate an area within Fines Drive Park of equal to or greater than the area being used to accommodate roadway realignment.

A street closure requires City Council's approval, in accordance with Part III of *The Cities Act*.

Respectfully submitted,



Fred Searle, Director  
Planning & Development Services

Respectfully submitted,

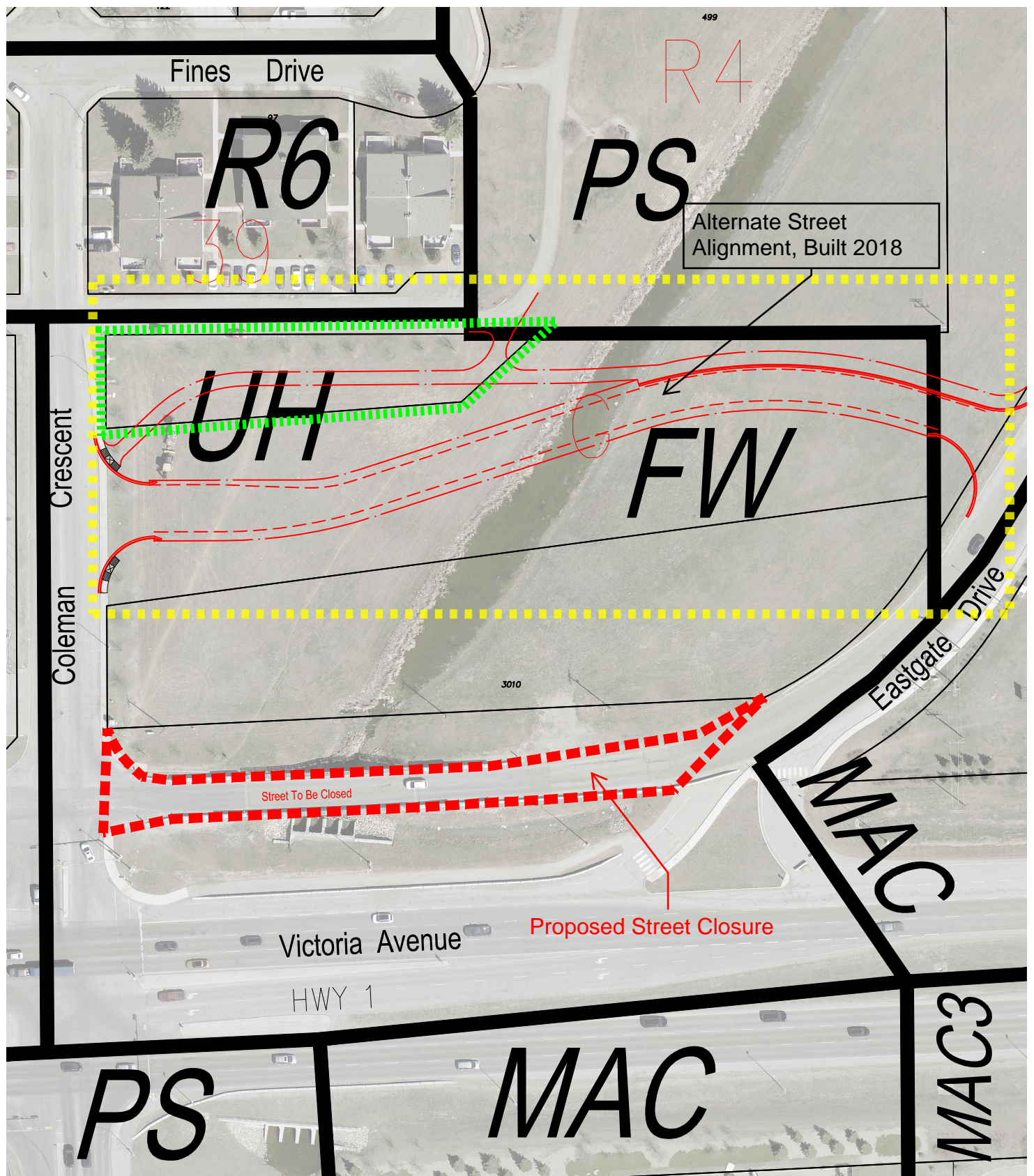


Diana Hawryluk, Executive Director  
City Planning & Community Development

Prepared by: Binod Poudyal, City Planner II

## ATTACHMENTS

Appendix A-1  
Appendix B



Date of Photography : 2018



Subject Property



Proposed Municipal Reserve



## Appendix B

### Public Consultation Summary

Response	Number of Responses	Issues Identified
<i>Completely Opposed</i>	-	-
<i>Accept if many features were different</i>	-	-
<i>Accept if one or two features were different</i>	-	-
<i>I support this proposal</i>	1	Initial concerns raised by the public have been fully addressed



## 15-OCP-03 Proposed Yards Neighbourhood Plan

<b>Date</b>	January 8, 2020
<b>To</b>	Regina Planning Commission
<b>From</b>	City Planning & Community Development
<b>Service Area</b>	Planning & Development Services
<b>Item No.</b>	RPC20-3

### RECOMMENDATION

---

Regina Planning Commission recommends that City Council:

1. Approve the application to amend *Design Regina: The Official Community Plan Bylaw No. 2013-48* by adding the Yards Neighbourhood Plan, attached as Appendix C, as Part B.18.
2. Direct the City Solicitor to prepare the necessary bylaw to amend *Design Regina: The Official Community Plan Bylaw No. 2013-48* by adding the Yards Neighbourhood Plan, attached as Appendix C, as Part B.18.
3. Approve these recommendations at its January 29, 2020 meeting, to allow sufficient time to advertise the required public notice for the respective bylaw.

### ISSUE

---

The application proposes to amend the *Design Regina: The Official Community Plan Bylaw No. 2013-48* (OCP) by adding Yards Neighbourhood Plan as Part B.18 to the OCP. The Yards Neighbourhood Plan sets the policy framework for subsequent planning endeavours, including a concept plan, rezoning, subdivision, as well as public investments.

The proposal supports the goals of *Design Regina: The Official Community Plan Bylaw No. 2013-48* (OCP).

## **IMPACTS**

---

### **Policy / Strategic Impact**

The recommendations support OCP goals by providing opportunities for a full range of mixed use: residential, commercial, institution and civic activities and supporting intensification, development of a complete neighbourhood as well as maintaining and enhancing the City Centre as the primary civic and cultural hub.

The Regina Revitalization Initiative (RRI) integrates the former intermodal lands into the City Centre by adding an estimated population of 2,500 new residents to achieve the OCP's target population of at least 10,000 new residents within the City Centre as outlined in OCP Policies 7.8.2 and Map 1 Growth Plan. Additionally, the RRI also incorporates vision and principles approved in the report CR11-99 approved by the City Council on August 22, 2011 which recommends that "Over the next 10 – 15 years, the Regina Revitalization initiative will realize a generational opportunity to revitalize the heart of our city by converting industrial and underused lands into a vibrant and energetic residential, commercial and retail development".

### **Financial**

Future application for concept plan and subsequent development permit will require demonstration of servicing implication and costs of any additional infrastructure or changes to existing infrastructure that may be required to directly or indirectly support the development, in accordance with City of Regina (City) standards and applicable legal requirements. The municipal infrastructure that is built and funded by the developers will become the City's responsibility to operate and maintain through future budgets.

### **Environmental**

The policies within the proposed Yards Neighbourhood Plan require that further environmental, geotechnical and quantitative risk analysis be submitted for review prior to concept plan, rezoning and development permit approval. A noise and vibration study has been submitted with recommendations to mitigate the impact of rail facilities on adjacent future land uses. The proposed Yards Neighbourhood Plan incorporates the recommendations of the study by including the policy for landscape buffer and setback requirements for building from the property line adjacent railway line.

## **OTHER OPTIONS**

---

Administration recommends approval of Yards Neighbourhood Plan.

There are two alternative options to the recommendation of approval:

1. Deny the proposal – Development will not occur on the subject property if City Council denies the application. Administration would be required to draft and submit a new plan for City Council's approval before the site may be developed.
2. Refer the proposal back to the Administration. If City Council has specific concerns with



the proposal it may refer back to the Administration for further review. Referral of the report back to the Administration will delay approval of the Yards Neighbourhood Plan until requested information has been gathered or changes to the proposal have been made. This would result in delay of approval depending on the scale and nature of the request.

## **COMMUNICATIONS**

---

The applicant and other interested parties will receive a copy of the report and notification of the meeting to appear as a delegation, in addition to receiving written notification of the Council's decision.

## **DISCUSSION**

---

As a part of the RRI, the City of Regina's Land and Real Estate Management Department has applied to amend the OCP by adding Yards Neighbourhood Plan as Part B.18 to the OCP. This Plan will guide redevelopment and transformation of a portion of the former Canadian Pacific Railway (CPR) intermodal yard, along with redevelopment of Dewdney Avenue between Albert and Broad Streets and addresses opportunities to connect Downtown to the Warehouse District. This Plan provides the policy framework to consider more detailed planning endeavors to realize the Plan, including a concept plan, zoning regulations, subdivision and development approvals, as well as capital improvements. The proposed Yards Neighbourhood Plan provides a strategy and flexibility to accommodate mix of residential, commercial, open space, sports and recreational and institutional development within an urban and walkable environment.

The process to develop this Yards Neighbourhood Plan has included an analysis of servicing potential. The proposed transportation network can accommodate projected traffic. The future concept plan shall demonstrate servicing capacity and, that the development can proceed without having a significant impact on existing water and wastewater service levels.

The Yards Neighbourhood Plan has been prepared in conformity with the City's requirements for stakeholder engagement and analysis, and generally aligns with the OCP. Letters were mailed out to property owners in the vicinity, and a public open house was held for the public to participate and provide comments and feedback. Regina Warehouse and Downtown Business Improvement Districts were engaged. A more detailed accounting of the respondent's comments and Administration's response is provided in Appendix B.

### **Land-use and Built Form Overview**

The Plan Area consists of 26 hectares (64 acres) on land, bordered by Albert Street to the west, Dewdney Avenue to the north, Broad Street to the east and the CPR mainline to the south, as shown in Appendix A-1. Policy areas A, B, C, and D are identified within the Plan Area as shown on Appendix A-2. Area A consists of existing commercial development, which is largely built out, but has potential for intensification. The existing development will be allowed continue; however, any new developments shall conform to the policies within this Plan. The balance of the site identified as Areas B, C and D, occupies seven hectares (17 acres) of land, and consists primarily of the (former) railyard site, which is vacant. Area

B within the railyard site allows for flexibility to be developed as recreational, sports entertainment facilities and/or convention centre or any other compatible land-uses including mixed use residential and commercial in the future. Area C and D allows for the development of parks and mixed-use residential and commercial development. A copy of the “Yards Neighbourhood” land-use map is attached to this report as Appendix A-2

A primary objective of this Plan is to ensure that the built form transitions from the Downtown to the Warehouse District and that appropriate interface design is established. As such, this Plan requires that taller buildings be located along the south side of the Plan Area and adjacent to the pedestrian bridge and park. This is intended to ensure that buildings support an active public realm and appropriate transitions between building heights and massing (Appendix A-6). The Plan requires that any future developments along Dewdney Avenue shall confirm to height and massing guidelines set forth in this Plan and be further enhanced through a Direct Control District zoning regulation which can set specific design and development criteria to this site.

### **Mixed use residential and commercial development**

The majority of the lands within the Plan Area are identified as the railyard site as outlined in the land use map [(Area B, C & D) Appendix A-2] accommodating future mixed-use development, including multi residential development. This Plan prohibits large format commercial, commercial uses that require large areas of outdoor storage and outdoor manufacturing, drive-thru restaurants, gas stations, and industrial uses. This Plan advances the goal of reaching a future residential population of up to 2,500 within the Regina Revitalization Initiative (RRI) lands as outlined in OCP Map 1 – Growth Map.

### **Railway Proximity**

The applicant submitted a noise and vibration study for review and, based on that report, this Plan identifies the need to set specific parameters and policies to mitigate any risks associated with railway proximity. The Plan outlines the requirement of a safety buffer by having 30-meter setback from the south property line along the main track running east-west and 15-meter setback from the property line along the north-south line as a requirement for all building excluding accessory use or non occupancy building such as parkades. The landscape buffer shall be built as a combination of an earthen berm and sound attenuation fence within the setback for any residential development near the railway lines (Appendix A-3). This is aligned with Federation of Canadian Municipalities and Railway Association of Canada guidelines for the land uses in proximity to rail corridor.

The main purpose of the berm and the fence is to act as a safety buffer and mitigate any noise and vibration being generated by the moving locomotive. The Plan also requires that any landscape buffer shall be built in accordance with the recommendation provided by the noise and vibration study.

### **Parks and Open Space**

Specific details of the Parks and Open Space will generally be identified through the concept plan process. However, the Yards Neighbourhood Plan provides general direction respecting major features such as landscaped green space, plazas or as a combination thereof and landscape buffers along the railway corridor (Appendix A-3).

## Transportation and Circulation

Major components of the transportation and circulation system will include redevelopment of Dewdney Avenue to match the intended urban form by enhancing pedestrian oriented urban development. It will form a core component of the transportation system to a multi-modal landscaped boulevard that accommodates pedestrians, cyclists, transit and vehicles (Appendix A-5); a pedestrian bridge crossing the CPR corridor will also form a core component of the transportation system and public realm, linking the Downtown to the Plan Area; enhanced Albert Street and Broad Street underpasses; and an internal street network. The Plan provides high-level direction for the internal streets only, as the detailed solution will be identified through an approved concept plan (Appendix A-4).

## Implementation

This Plan provides high-level policy direction for the growth, development and servicing of the Plan Area, as well as direction for more detailed planning and engineering work. Prior to development occurring, a concept plan must be prepared and approved by the City Council, followed by rezoning. The concept plan and zoning designation must be in conformity with this Plan.

Additionally, the Plan also outlines further requirements in regard to any environmental risks as well as complete quantitative risk assessment to mitigate any risks associated with the Railway site prior to approval any concept plan(s).

Administration recommends approving the application based on the following rationale:

1. The proposed Plan is consistent with the OCP.
2. The proposed Plan Area is well positioned for a mixed-use residential and commercial development due to its proximity to downtown Regina and will provide opportunities to materialize RRI visions and principles.

## DECISION HISTORY

---

Amendments to the OCP require City Council's approval, in accordance with Part IV of *The Planning and Development Act, 2007*. Ministerial approval is required before amendments to the OCP come into force.

Respectfully submitted,



Fred Searle, Director  
Planning & Development Services

Respectfully submitted,



Diana Hawryluk, Executive Director  
City Planning & Community Development

Prepared by: Binod Poudyal, City Planner II

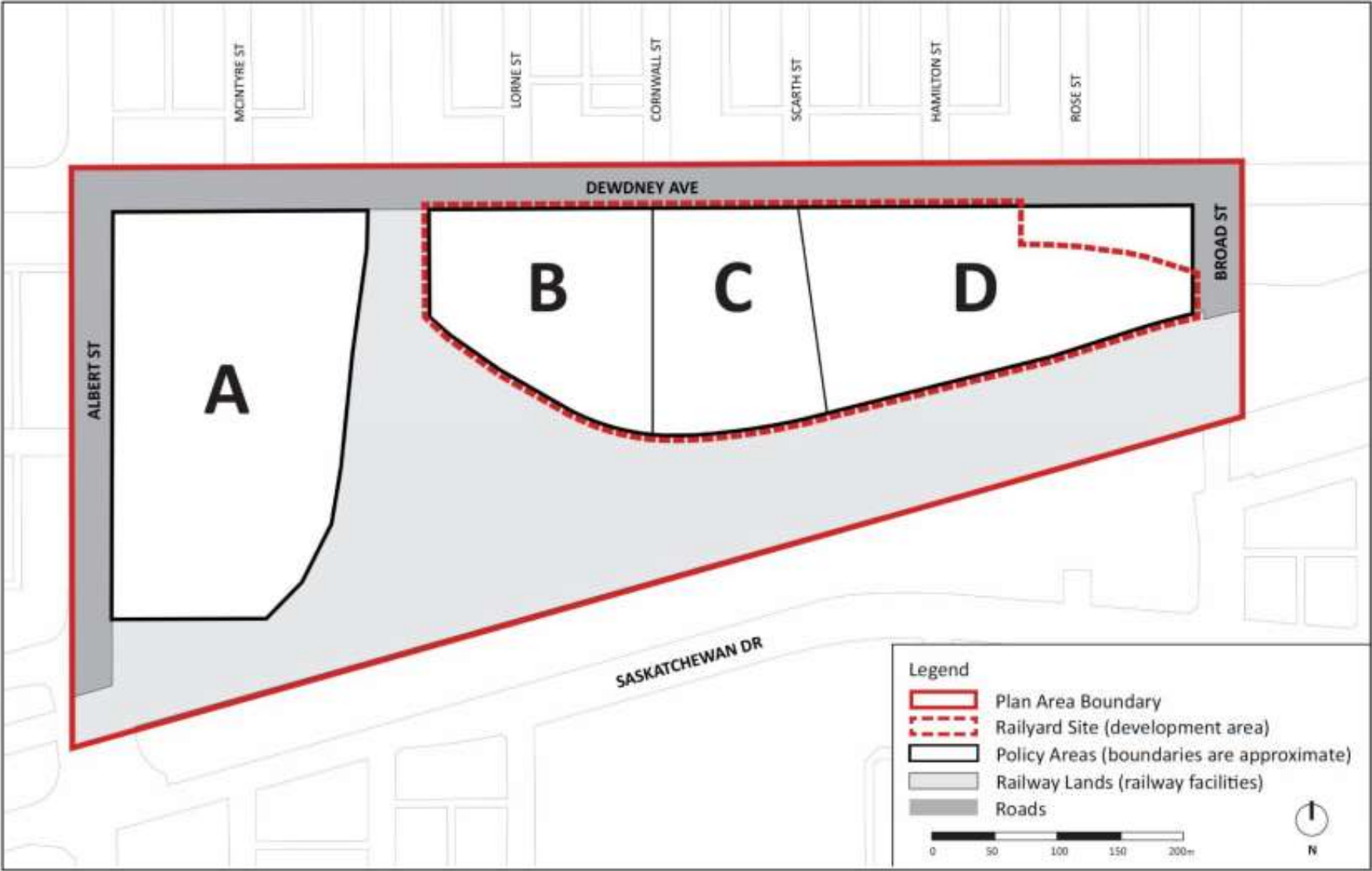
## **ATTACHMENTS**

Appendix A-1 (Location Map)  
Appendix A-2 (Proposed Land-use Map)  
Appendix A-3 (Proposed Railway Setback and Landscape Buffer)  
Appendix A-4 (Proposed Mobility Plan)  
Appendix A-5 (Proposed Dewdney Avenue Redevelopment)  
Appendix A-6 (Proposed Building Heights)  
Appendix B (Public Open House Summary)  
Appendix C (Proposed Area Plan)

## Appendix A-1 Location Map

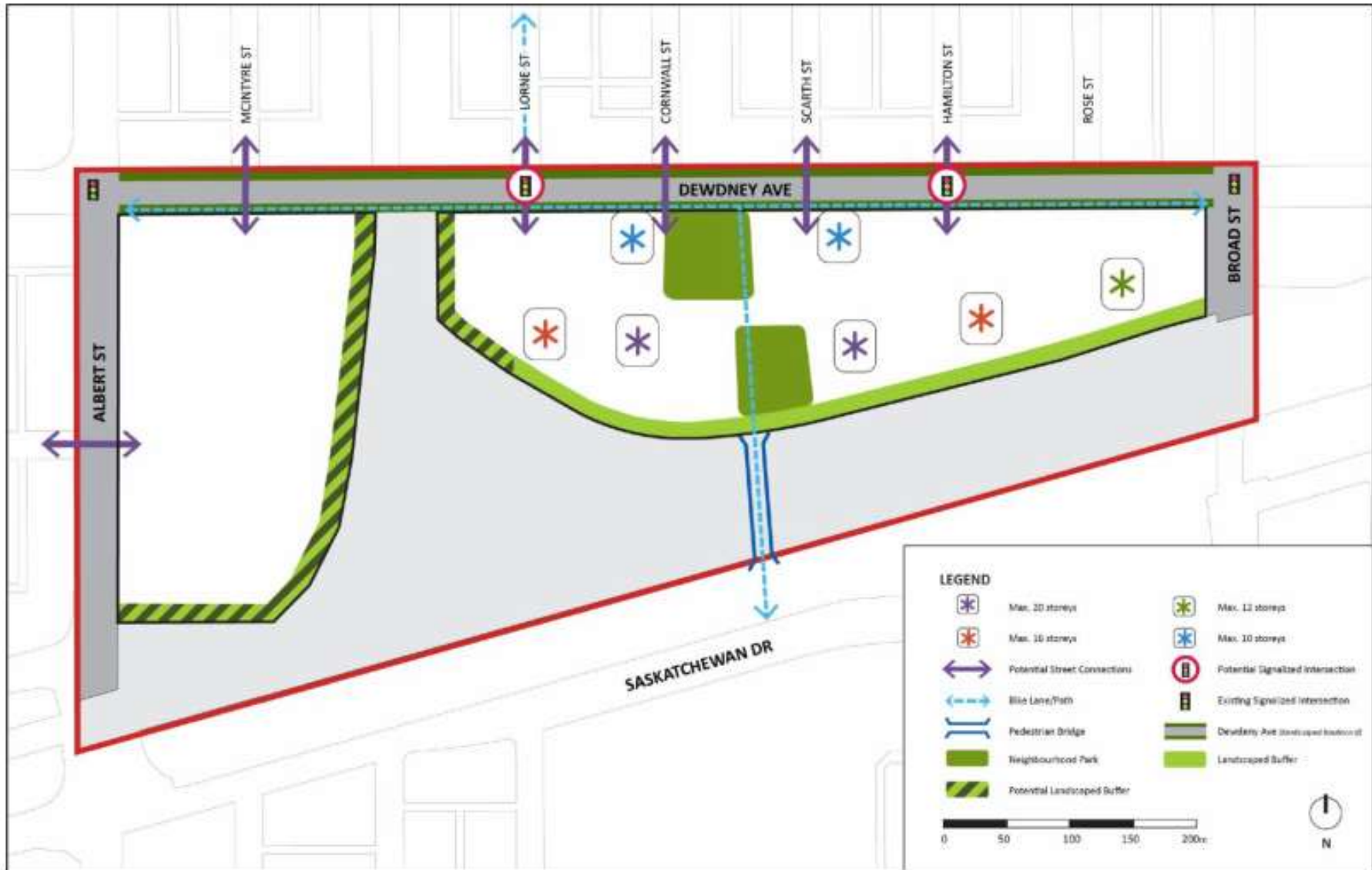


Appendix A-2 Land use Map

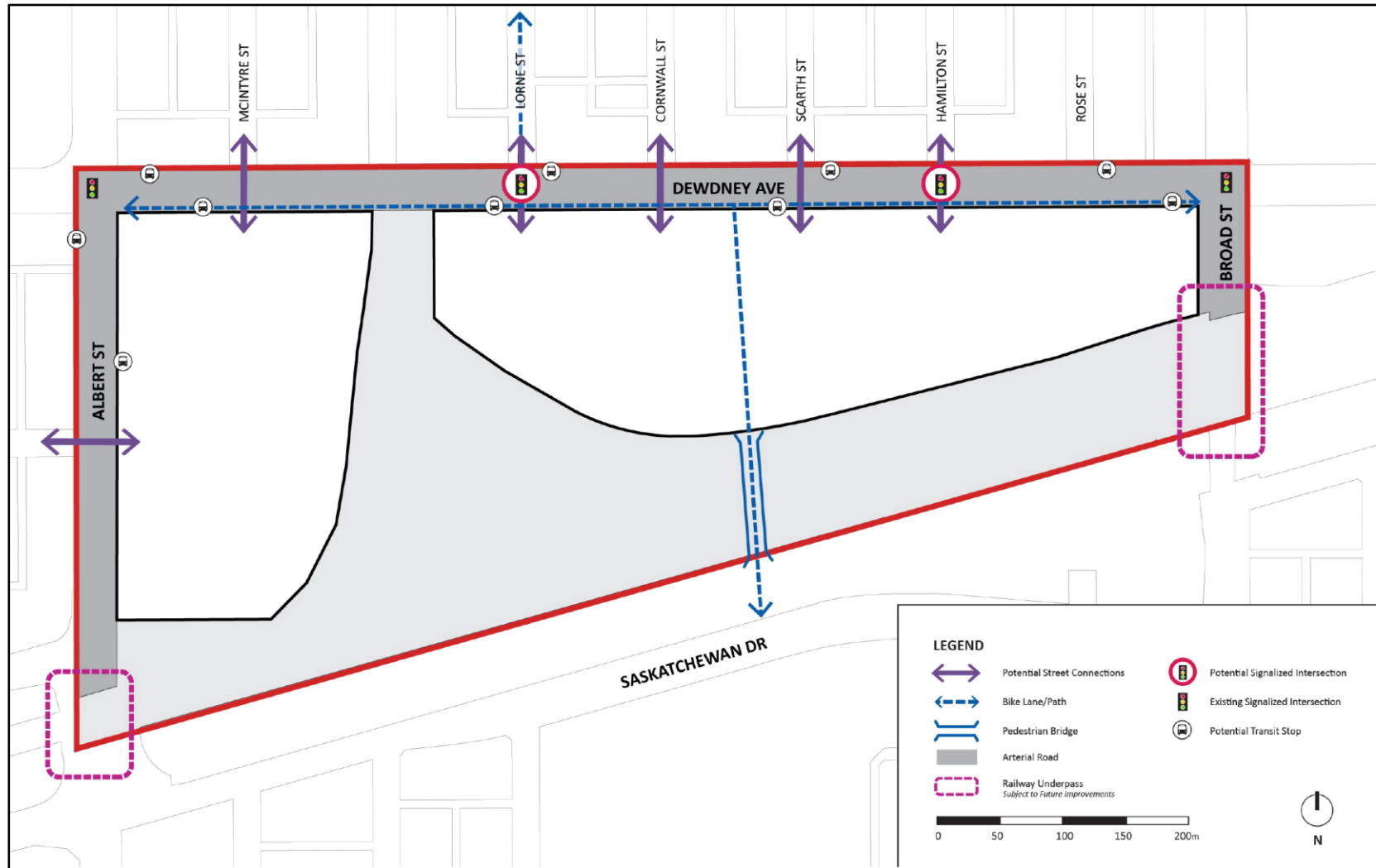




## Appendix A-3 - Proposed railway setback and Landscape Buffer

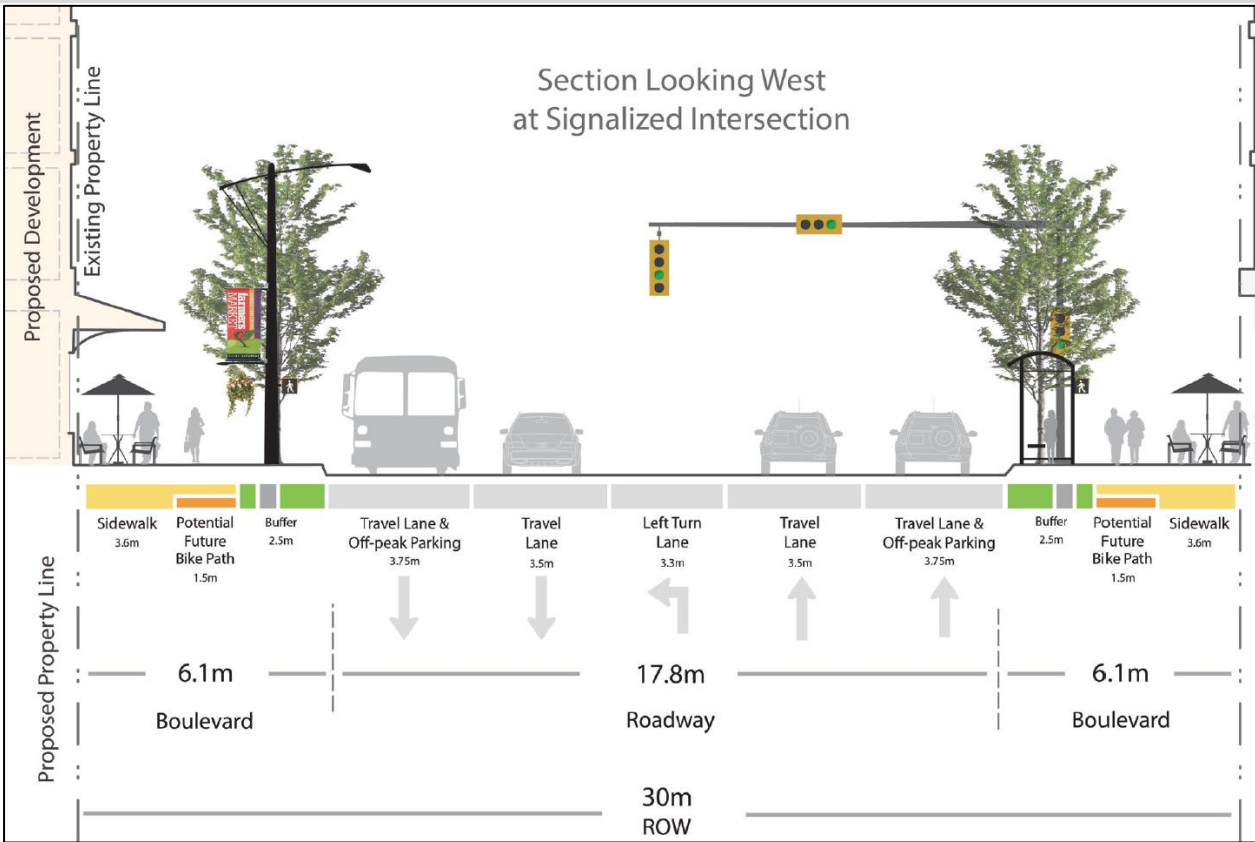
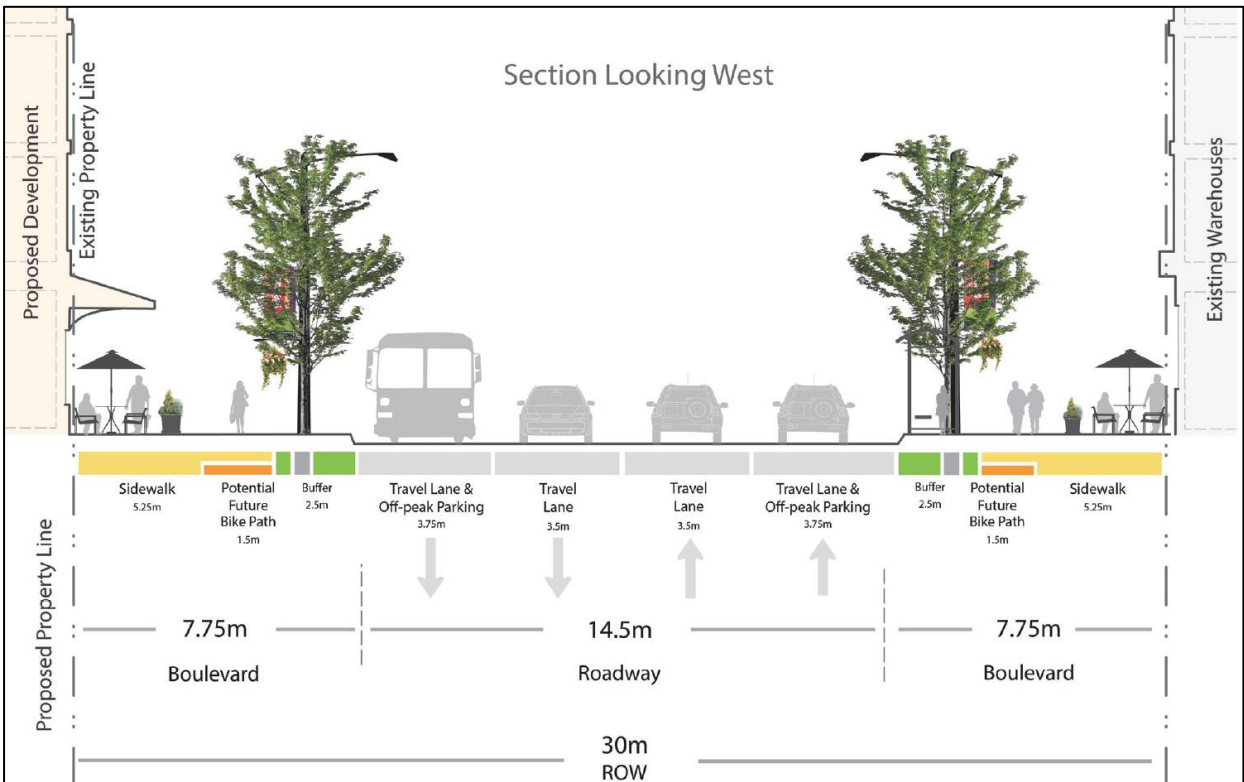


## Appendix A-4 Proposed Mobility Plan

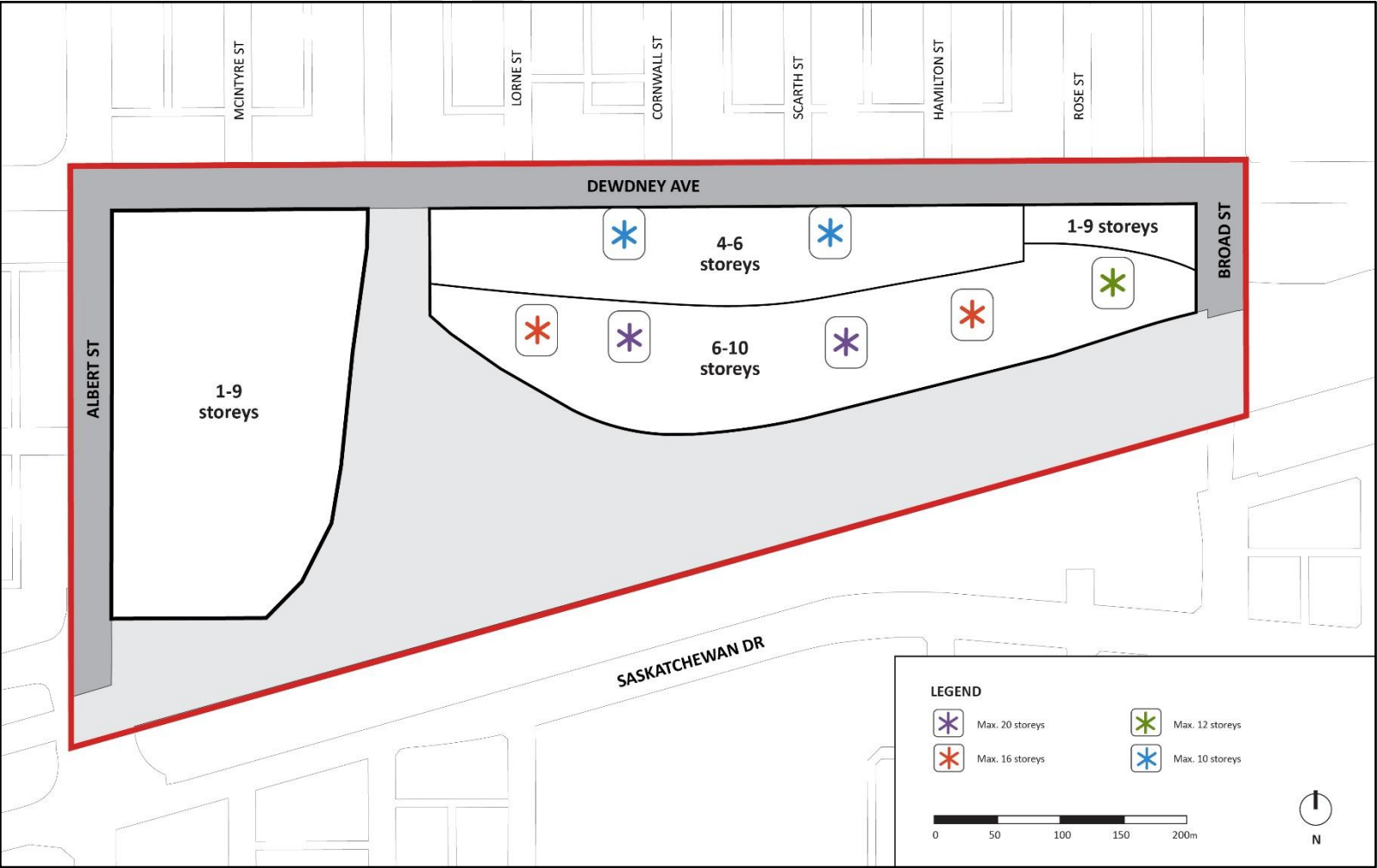




Appendix A-5 Proposed Dewdney Avenue Redevelopment



Appendix A-6 Proposed Building Height



**Public Consultation Summary**

Response	Number of Responses	Issues Identified
<i>Completely Opposed</i>	2	<ul style="list-style-type: none"> <li>• These financial investments are not beneficial to most of the people and the funding could be used for emergency services, homes for disabled and homeless or to increase employment</li> <li>• Area could become a place for vagrants to occupy</li> <li>• Development is too close to the Railway corridor, mainly residential</li> </ul>
<i>Accept if many features were different</i>	3	<ul style="list-style-type: none"> <li>• A Better downtown hub for transit and accessible structured buildings</li> <li>• Residential and commercial development may have negative impacts to downtown.</li> <li>• Plan for lesser number of taller buildings than proposed</li> <li>• Focus should be on parks rather than housing</li> </ul>
<i>Accept if one or two features were different</i>	4	<ul style="list-style-type: none"> <li>• Should be pedestrian oriented development</li> <li>• Pedestrian connection to warehouse district should be the principal of the plan</li> <li>• Proposed Dewdney avenue development concept may increase traffic may impact plan's intent to create an area "suitable for pedestrian strolling and sidewalk shopping."</li> <li>• Narrowing Dewdney avenue to 4 lanes may have issues for emergency vehicles</li> <li>• Design wider sidewalks along Dewdney Avenue and Bike path, if used should be separate from the sidewalk</li> <li>• Add recreational opportunities, space and equipment's for children such as skateboard park and playground and a major public art piece</li> <li>• Add public facilities such as library, farmer's market, Rink, community centre</li> <li>• Limit the height of the buildings to 3 stories as the taller buildings block the sun during winter.</li> <li>• Taller buildings may interfere with views of downtown from the warehouse</li> <li>• What will happen to the existing Spur line</li> </ul>
<i>I support this proposal</i>	7	<ul style="list-style-type: none"> <li>• Development of the Yards in general</li> <li>• Overall vision for infill development with mixed use, park and connection to downtown</li> </ul>

		<ul style="list-style-type: none"> <li>• Enhancement of Warehouse district</li> <li>• Pedestrian connection to downtown and enhancement to existing connection at Broad and Albert Street</li> <li>• Vibrant urban neighbourhood</li> <li>• The Plan is very positive, Pedestrian friendly, allows for mixtures of uses</li> <li>• Dewdney Avenue redevelopment concept</li> <li>• Park space, landscape buffer and restricted parking</li> <li>• Variation in building heights, with lower height along Dewdney avenue and gradually increasing towards the downtown</li> </ul>
--	--	--

**1. Issue:**

Area could become a place for vagrants to occupy

*Administration's Response:*

This area once developed, will be a vibrant urban realm with multiple residential and commercial mixed-use development along with public parks and a potential arena or any other compatible uses.

**2. Issue:**

Development is too close to the Railway corridor, mainly residential

*Administration's Response:*

During the review process, administration reviewed reports pertaining to Noise and Vibration impact of the adjacent railway line. Based on those study, this report incorporated policies and recommendation for minimum building setback and landscape buffer. These recommendations align with the federation of Canadian municipalities and the railway association of Canada's guidelines for new development in proximity to railway operations.

**3. Issue:**

These are not financially beneficial investments

*Administration's Response:*

Development of the Yards Neighbourhood plan is an outcome of the City Council's vision and principles for Regina Revitalization Initiative and confirms to the Design Regina: *Official Community Plan*.

**4. Issue:**

Residential and commercial development may have negative impacts to downtown Business.

*Administration's Response:*

Every location has its own competitive advantages, and the same may be true for the Yards as well. The future development should present a positive competitive challenge for both the downtown and the Yards allowing businesses to succeed.

#### **5. Issue**

A Better downtown hub for transit and accessible structured buildings

*Administration's Response:*

The plan currently outlines Dewdney Avenue as a pedestrian and street-oriented development.

#### **6. Issue**

Focus should be on parks rather than housing

*Administration's Response:*

The proposed Plan outlines confirms to OCP's goal of a complete neighbourhood where multitude of residential and commercial development are planned along with a landscape buffer and public park space.

#### **7. Issue**

Narrowing Dewdney avenue to 4 lanes may have issues for emergency vehicles

*Administration's Response:*

The proposed Dewdney avenue redevelopment will be planned and designed accordingly to the municipal design standards, which requires accessibility for all emergency vehicles. Similarly, based on the studies completed by the applicant, the proposed configuration will help ease the vehicle speed travelling through Dewdney Avenue.

#### **8. Issue**

Design wider sidewalks along Dewdney Avenue and Bike path, if used should be separate from the sidewalk

*Administration's Response:*

The figure outlining the redevelopment of Dewdney avenue is conceptual only, the design of Dewdney avenue must confirm to the City's design standards process

#### **9. Issue**

Add public facilities such as library, farmer's market, rink, community centre

*Administration's Response:*

The proposed land use plan supports the potential for two specialized area: a central public/ civic area that could include a neighbourhood park and civic buildings: an "entertainment area" that could include an appropriately scaled sports, entertainment or convention centre.

## **10. Issue**

Add recreational opportunities, space and equipment's for children such as skateboard park and playground and a major public art piece

### *Administration's Response:*

Detail design of the proposed park shall be established only after a requirement analysis is completed at the time of concept plan application and shall confirm to the City of Regina Recreation Masterplan - 2019.

## **11. Issue**

Limit the height of the buildings to 3 stories as the taller buildings block the sun during winter. Taller buildings may interfere with views of downtown from the warehouse

### *Administration's Response:*

Majority of the buildings along Dewdney Avenue have been restricted to 4-6 along Dewdney Avenue and 6-10 stories along the Railway corridor, this should allow for a smooth transition and be compatible with the development along north side of Dewdney as well as downtown Regina.

The policy allows for taller buildings at several location in order to meet the population target of 2,500 between this development and the old Taylor field as outlined in the *Official Community Plan* Map 1 – Growth plan

## **12. Issue**

Future Developments in the Yards should be subject to same policies and fees applied to rest of the city.

### *Administration's Response:*

Yards Neighbourhood Plan is a policy document to help guide the future development, future incentives development incentives (if any) have not been included in the plan. The methodology for such incentives could be developed and be incorporated within the zoning regulation. Such insensitive could be like the ones outlined in the Zoning Bylaw 2019.



November 28, 2019

Binod Poudyal  
City Planner II  
Planning and Development Services Department  
City of Regina  
2476 Victoria Avenue  
Regina, SK S4P 3C8

**Re: Proposed Yards Neighbourhood Plan (File No. 150CP03)**

Dear Mr. Poudyal:

Thank you for the opportunity to comment on the proposed Yards Neighbourhood Plan on behalf of Regina Downtown Business Improvement District (RDBID).

Regina Downtown Business Improvement District is encouraged to see the inclusion of a major recreation, sport, entertainment facility and convention centre as permitted uses in Mixed-Use Policy Area B and as such, offers conditional support for the proposed Yards Neighbourhood Plan. Our support is conditional on future development within the Yards being subject to the same policies and fees applied to developments elsewhere in the City. RDBID objects to any future development incentives being offered on the Yards site.

The Yards Neighbourhood is an exciting opportunity for the City of Regina, however, we caution against the replication of successful uses currently being offered in other areas of the city. This would create the risk of cannibalization of other prosperous neighbourhoods.

Densification should be encouraged in our city centre. It is our hope that policies and plans that support densification, and not necessarily replication, will lead to private investment and residential development in the heart of Regina.

Should you have any questions, please do not hesitate to contact me at 306.359.7573.

Thank you.

Sincerely,

A handwritten signature in black ink, appearing to read "Judith Veresuk".

Judith Veresuk  
Executive Director  
Regina Downtown Business Improvement District



REGINADOWNTOWN.CA



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## Regina's Warehouse Business Improvement District

Ph. 306.585.3948  
Fx. 306.585.1765

202-1275 Broad St  
Regina, Sk S4P 3B2

info@warehousedistrict.ca  
www.warehousedistrict.ca



December 3, 2019

Binod Poudyal  
City Planner II  
Planning and Development Services Department  
City of Regina  
2476 Victoria Avenue  
Regina, SK S4P 3C8

Re: Proposed Yards Neighbourhood Plan (File No. 150CP03)

Dear Mr. Poudyal:

Thank you for the opportunity to provide comments on the proposed Yards Neighbourhood Plan.

Regina's Warehouse Business Improvement District is excited to see the development of the former railyard site — it isn't often that a city has such a golden opportunity to redevelop an area within its city centre. Development should complement the existing city centre, create a bridge between Downtown with the Warehouse District, and ensure a pedestrian and bike friendly plan that establishes the vital connection between The Yards with Dewdney Avenue.

RWBID is encouraged to see the zoning allow for the inclusion of a major recreation, sport, entertainment facility and convention centre as permitted uses in Mixed-Use Policy Area B and as such, offers conditional support for the proposed Yards Neighbourhood Plan.

We would be interested in exploring incentives to encourage the development of underground parking and to attract a major entertainment/convention/sport facility.

An increase in density in the area done with thoughtfulness and forward thinking will help lead to a vibrant city centre. We look forward to continued discussions on the development of the Railyard Renewal Project.

Kind Regards,

Leasa Gibbons  
Executive Director





# Yards

## Neighbourhood Plan

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### OCP – Part B.18

**Version: November 28, 2019**

## PART B.18

### Yards Neighbourhood Plan

Enactment			
		Date	Bylaw
Approved	City of Regina		
	Government of Saskatchewan		N/A
Amended			

This Neighbourhood Plan forms part of:

***Design Regina: The Official Community Plan Bylaw No. 2013-48***

Enactment of this Neighbourhood Plan is authorized through Section 29 of:

***The Planning and Development Act, 2007***

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## EXECUTIVE SUMMARY

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This Neighbourhood Plan (Yards Neighbourhood Plan) sets forth a policy framework for transitioning the former Canadian Pacific Railway (CP Rail) intermodal yard, and adjacent lands, located in Regina's City Centre, into a vibrant urban neighbourhood. The Yards Neighbourhood will include a mix of residential, shopping and entertainment uses set within a compact and walkable environment and will balance progressive and modern design concepts with tradition. Strategically located, the Yards Neighbourhood will better connect the Warehouse District with Downtown, thus bridging and strengthening Regina's City Centre.

The Yards Neighbourhood Plan forms part of *Design Regina: the Official Community Plan Bylaw No. 2013-48* (Design Regina OCP). As a policy document approved by Regina City Council, the Yards Neighbourhood Plan provides direction for future land-use, development and servicing. The policies set forth are oriented towards achieving a long-term vision, while remaining flexible in order to support evolving market conditions and investment opportunities overtime. Beyond the Yards Neighbourhood Plan, more detailed planning will occur through the preparation of a concept plan and through the rezoning, subdivision and development processes.

Development of the Yards Neighbourhood will occur incrementally, starting with site improvements and capital upgrades. Major improvements may include:

- Transformation of Dewdney Avenue into a multi-modal landscaped boulevard.
- The construction of a landscaped buffer, and amenity, along the railway corridor.
- Upgrades to major utility infrastructure.
- The installation of a pedestrian bridge, connecting to downtown.
- Upgrades to the railway underpasses at Albert Street and Broad Street.

The preparation of the Yards Neighbourhood Plan has been a highly collaborative process. The City is most appreciative to the thousands of members of the public, stakeholders and landowners who gave generously their time, energy and input to make the Yards Neighbourhood Plan possible. The City Administration and Council will continue to engage with citizens, stakeholders and landowners to realize the vision and values of this plan.

## **1. INTRODUCTION**

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### **1.1. Background**

In 2012, CP Rail decided to relocate and expand its intermodal freight rail yard from the historic City Centre location to the new Global Transportation Hub (GTH), located in the west part of the city. Recognizing the once-in-a-generation opportunity to enhance and vitalize the Downtown and City Centre, the City of Regina (City) purchased the intermodal freight rail yard (Railyard Site) as the first step towards infill and redevelopment. The “Railyard Renewal Project”, forms part of a larger “Regina Revitalization Initiative” (RRI). The RRI also includes the new Mosaic Stadium, which opened to the public in 2016, and the redevelopment of the former Taylor Field Stadium site, which is a future land development project.

The planning process for the Railyard Renewal Project began in 2015 with the establishment of a project steering committee and the hiring of consultants to advise on planning and engineering. The planning process has involved four phases of work:

- Phase 1: Analysis of opportunities and challenges associated with the Plan Area.
- Phase 2: Preparation of three potential development scenarios.
- Phase 3: Review and selection of preferred development scenario.
- Phase 4: Preparation of Yards Neighbourhood Plan.

The planning process has included extensive engagement and analysis. Engagement has included consultation with the public, stakeholders and two advisory committees consisting of adjacent landowners and representatives of the Downtown and Warehouse Business Improvement Districts (BIDs) and other stakeholders. Public engagement has also included four open house events, two design charrettes and several on-line surveys. Analysis has included geotechnical investigations, environmental site analysis, servicing analysis and market research.

The culmination of this initial planning process is the the Yards Neighbourhood Plan – a policy document that will direct future land-use, development and servicing. The Yards Neighbourhood Plan responds to the unique nature of the Railyard Site, as a former intermodal freight rail yard, and advances the vision and aspirations of Regina City Council, citizens and stakeholders.

## **1.2. Regulatory Context**

### Planning and Development Act, 2007

The Yards Neighbourhood Plan is a type of secondary plan and forms part of Design Regina OCP. *The Planning and Development Act, 2007*, which is a statute of the Government of Saskatchewan (Province), provides the authorization for a municipality to enact an official community plan, and stipulates what an official community plan must and can address. Official community plans, and the process to adopt or amend an official community plan, must be in conformity with *The Planning and Development Act, 2007* and the associated *Statements of Provincial Interest* regulations, which defines matters of provincial interest.

### Official Community Plan (“Design Regina”)

The Yards Neighbourhood Plan is included within Part B of Design Regina OCP. Official community plans are policy instruments used by municipalities to guide, over a long-term period, growth, development, the provision of services, and other matters, across the municipality. Whereas Part A of Design Regina OCP provides general policy direction for the city as a whole, the secondary plans contained in Part B apply to specific sub-areas within the city (e.g. new neighbourhoods). As a secondary plan, the Yards Neighbourhood Plan must be in conformity with Part A.

An important element of Design Regina OCP is the direction it provides respecting growth planning and phasing. Maximizing the efficient use of infrastructure and supporting “complete neighbourhoods”, through infill development, is a significant objective. To this end, it is the aim of Design Regina OCP to direct approximately 10,000 additional people to the City Centre, including approximately 2,000 people to the Plan Area (Design Regina OCP – Map 1). This Neighbourhood Plan, therefore, significantly advances the growth planning objectives of Design Regina OCP.

### Concept Plans, Rezoning and Subdivision

As a general requirement for rezoning and subdivision approval, a concept plan must be prepared and approved for specified development areas. Concept plans illustrate the specific location of land-use, open space and transportation networks and must be in general conformity with this Plan. Likewise, rezoning and subdivision approval will generally be required as a prerequisite for development and these applications must conform with an approved concept plan.

### **1.3. Plan Interpretation**

#### Policy Interpretation

In the interpretation of the policies within this Plan, the word:

- “Shall” equates to mandatory compliance.
- “Should” infers that compliance is generally expected, except where execution of the policy is not practical or where an exceptional situation applies, etc.
- “May” infers that execution of the policy is optional; however, where “may” is used in conjunction with a City directive, the City has final authority to require or waive execution of the policy.

#### Reference Interpretation

- “Yards Neighbourhood Plan” refers to this policy document, which is Part B.18 of Design Regina OCP, and is abbreviated herein as either “Neighbourhood Plan” or “Plan”.
- “Plan Area” refers to the geographic area subject to the policies of this Plan, as described in Section 2.1. Policies and requirements of this Plan apply to the Plan Area only.
- “Railyard Site” refers to the former CP Rail intermodal freight rail yard.
- “Yards Neighbourhood” refers to the future neighbourhood that will emerge in the Plan Area, following the redevelopment of the Railyard Site.

#### Map Interpretation

Unless otherwise specified within this Plan, the boundaries or locations of any symbols or land-use areas shown on a map are approximate only and are not intended to define exact locations except where they coincide with clearly recognizable physical features or fixed boundaries such as existing legal property lines, existing roads or existing utility rights-of-way. The precise location of land-use boundaries will be determined by the City at the time of concept plan, rezoning and subdivision applications. Where adjustments are made as a result of further delineation through the concept plan process, an amendment to the maps within this Plan shall not be required.

#### Use Interpretation

To provide general direction respecting the intended use and development of areas throughout the Plan Area, the Plan references land-uses that may require interpretation. Within this Plan, when specific land-uses are mentioned, please refer to Design Regina OCP – Part A and/or Zoning Bylaw for further clarification. While this Plan provides broad policy direction relative to the intended use and development of an area, the ultimate definition and approval of land-uses shall be further delineated at the concept plan and rezoning stages.

## 2. PLAN AREA

### 2.1. Location

The area subject to the policies of the Yards Neighbourhood Plan (Plan Area) is 26 hectares (64 acres) in size and is situated between Dewdney Avenue and the railway corridor (north-south) and between Albert Street and Broad Street (west-east). Within the Plan Area, the Railway Site, which is available for redevelopment, is 7 hectares (17 acres) in size. The balance of the Plan Area consists of existing commercial development and railway facilities.

The Plan Area is strategically located within the heart of Regina's City Centre, linking the Downtown and the Warehouse District. The Plan Area also forms part of Dewdney Avenue, which is a notable corridor, linking major institutional and cultural facilities. (Figure 1)

**Figure 1 – Location**





## **2.2. Site Character**

### Existing Uses

The Plan Area includes the Railyard Site; the existing railway corridors and existing commercial development. The Railyard Site is currently vacant, with all tracks and other structures having been removed. Within the west side of the Plan Area, between the north-south railway corridor and Albert Street, are existing commercial developments, including: a hotel, restaurants, multi-tenant commercial building. This existing commercial area is largely built-out; however, there are opportunities for infill and redevelopment. There are also existing commercial developments at the corner of Dewdney Avenue and Broad Street, including: commercial service and retail.

### Environmental

The Railyard Site has been subject to environmental evaluation, as they have accommodated, for a significant period, railway related infrastructure and storage. Testing included soil sampling and evaluation, focusing on potential contamination, and the identification of other potential impacts. Through this testing, some impacts have been identified, which will require remediation. Further, some areas may require additional, or more detailed, testing as a prerequisite for development. At the time this Plan was adopted, testing completed indicates that remediation is manageable, and impacts will not preclude development, pending remediation, where required. A health risk assessment and corrective action plan will be completed to determine remediation strategies for the Railyard Site.

### Railway Proximity

Together, the railway corridors and associated railway activities within the Plan Area may be a freight rail yard, including: an east-west oriented railway corridor, classified as a “main line”, a north-south oriented railway corridor, which is classified as a “branch line” and a railcar maintenance area. Although the transfer of containers is no longer performed at this location, railcars are regularly stored and maintained. As these railway activities are anticipated to continue for the foreseeable future they have been evaluated from the perspective of safety and land-use compatibility.

While there are no setback requirements imposed by either the federal or provincial levels of government, as it relates to railway proximity, the strategy of this Plan is to require buffer areas and setbacks between the railway activities and development within the Plan Area. The primary buffer treatment required by the Plan is in the form of a landscaped buffer that includes a berm and a noise attenuation wall. The intent is that the berm will be well landscaped and will also serve as an open space amenity for the neighbourhood. Other forms of buffer treatment may include the placement of streets/ laneways and appropriate non-residential building (e.g. parking structures) next to railway corridors as a means of further reducing possible nuisance.

## 2.3. Site Context

### Warehouse District

The Plan Area is strategically located within the Regina City Centre, between two dynamic urban neighbourhoods: the Downtown and the Warehouse District. The predominant land use within the Warehouse District is light industrial and commercial retail; however, this eclectic area is transitioning over-time to include residential and mixed-use development. A core element within this neighbourhood is Dewdney Avenue, which serves as an entertainment and shopping destination, including restaurants, nightclubs and retail. The Warehouse District is well known for its historic buildings and unique character. Many of the buildings were built between 1910-1930 and reflect the “Chicago-Style” design: brick exterior; flat roof, rectangular window pattern.



### Downtown Regina

South of the Plan Area, across the railway corridor, is Downtown Regina, which serves as the main hub, within the city, for business, entertainment and civic activities. The Downtown includes: the majority of the city’s Class A and B office buildings, concentrated in the central business district; a mix of residential types and densities; shopping and entertainment, including an assortment of bars and restaurants, and amenities, such as the Victoria Park and Plaza and Casino Regina, etc. It is intended that the Downtown will continue to accommodate the majority of large-scale office development (e.g. Class A) and tallest residential built-form.



One objective of this Plan is to complement and support the Warehouse District and the Downtown. The residents of the Yards Neighbourhood will be within easy walking distance of both the Warehouse District and the Downtown, thus enhancing City Centre business activity by increasing the proximal customer base. The Warehouse District will be further enhanced through the full build-out of Dewdney Avenue, between Albert Street and Broad Street, thus “completing” this important segment of Dewdney Avenue. The Downtown, as well, will be further enhanced through new connections, such as the proposed pedestrian bridge, which will improve walkability and provide more convenient access between the Downtown and neighbourhoods to the north.

### **3. VISION & PRINCIPLES**

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#### **3.1. Vision**

It is envisioned that the Plan Area will transition, from its current state, into a vibrant urban neighbourhood, which includes a mix of residential, shopping and entertainment opportunities set within a compact and walkable environment. By balancing progressive and modern design concepts with tradition, the Plan Area will emerge as a truly unique neighbourhood, connecting the City Centre, and enhancing the City of Regina as a whole.

#### **3.2. Principles**

As set forth by Regina City Council (Council Report CR11-99), the Yards Neighbourhood shall be “mixed-use, diverse and environmentally sensitive”, in accordance with the following principles:

- A key city priority;
- A regional precedent that employs best practices in City-building;
- A vibrant and connected place to gather, live, work, learn, play, and celebrate year-round;
- An area in which the public realm has a high profile and inspires civic pride;
- Complementary to existing plans in surrounding neighbourhoods, yet sets a new standard for sustainable development, employing state of the art best practices for neighbourhood design and energy efficiency;
- A neighbourhood that will provide mixed-income housing options for purchase and rental;
- A community that will provide work space and mixed-use developments to foster innovation, creativity and cultural expression;
- Linked across the railway tracks to downtown by pedestrian connections that create walkable distances for residents, in four seasons, to local amenities and jobs;
- Characterized by high quality design, for all ages, and both programming and place making that promotes social cohesion and community activities; and
- An example of a new opportunity for the community being delivered in a way that is financially viable for the City, its residents, and businesses.

The vision and principles provide the foundation for the objectives and policies of this Plan. Beyond this Plan, the vision and principles shall be carried forward, in more detail, through the implementation of a concept plan and appropriate zoning code.

## **4. LAND-USE**

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### **4.1. Overview**

The intent of this section is to provide policy direction for the use of land within the Plan Area. In accordance with the vision and principles of this Plan, the policies herein support the development of a compact, walkable, mixed-use and sustainable neighbourhood. The policies establish direction for land-use requirements and also provide direction for subsequent planning initiatives, including the preparation of a concept plan and rezoning. These planning initiatives will advance the intent of this Plan by providing greater detail regarding land-use and design.

This Plan supports a mix of appropriate land-uses throughout, including a significant residential component. Non-residential land-uses will generally be located within the ground floor of mixed-use buildings. Although the predominant pattern is intended to be mixed-use, this Plan supports the potential for two specialized areas: a central public/ civic area that could include a neighbourhood park and civic buildings; an “entertainment area” that could include an appropriately scaled sports, entertainment or convention centre. This Plan also supports residential throughout and strives for a population of approximately 2,000 people.

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### **4.2. Policy**

- a) Within the Plan Area, the City shall only allow land-uses and development that are appropriate for a mixed-use, pedestrian-oriented urban environment, including: residential, commercial retail and services, office, civic, institutional, parks.
- b) Within the Plan Area, the City shall prohibit land-uses and development that are inappropriate for a mixed-use, pedestrian-oriented urban environment, including:
  - i. Uses that require large areas of outdoor storage or outdoor manufacturing.
  - ii. Uses oriented to automobile travel, such as drive-thru restaurants, gas stations.
  - iii. Large-format retail.
  - iv. Industrial uses
- c) Notwithstanding Policy 4.2(a) and 4.2(b):
  - i. Existing uses/ buildings that do not conform to this Plan may continue; however, at such time as they are replaced, the policies of this Plan shall apply.
  - ii. The City may consider, within Mixed-Use Policy Area B, as shown on Figure 2, the following land-uses: recreation, sports, entertainment facility; convention centre; or similar.
  - iii. Within the Railyard Site area, as shown on Figure 2, the City may consider outdoor uses (e.g. surface parking; outdoor events) on an interim basis, until such time as the Railyard Site is rezoned and/ or developed in accordance with the vision, principles and policies of this Plan.

- iv. The ground-floor of buildings fronting Dewdney Avenue and Albert Street should consist of commercial uses or other non-residential uses supported by this Plan.
- d) Land adjacent to the proposed Neighbourhood Park, as shown on Figure 5, shall be reserved for civic or institutional buildings; however, should it be determined by the City, at any time, that civic or institutional buildings are not viable or desirable, the land may transition to an alternate acceptable land-use without an amendment to this Plan.
- e) Where a building flanks the proposed Neighbourhood Park, as shown on Figure 5, the ground-floor should front the Neighbourhood Park and consist of commercial uses or, at the City's discretion, other "active" and appropriate non-residential uses.

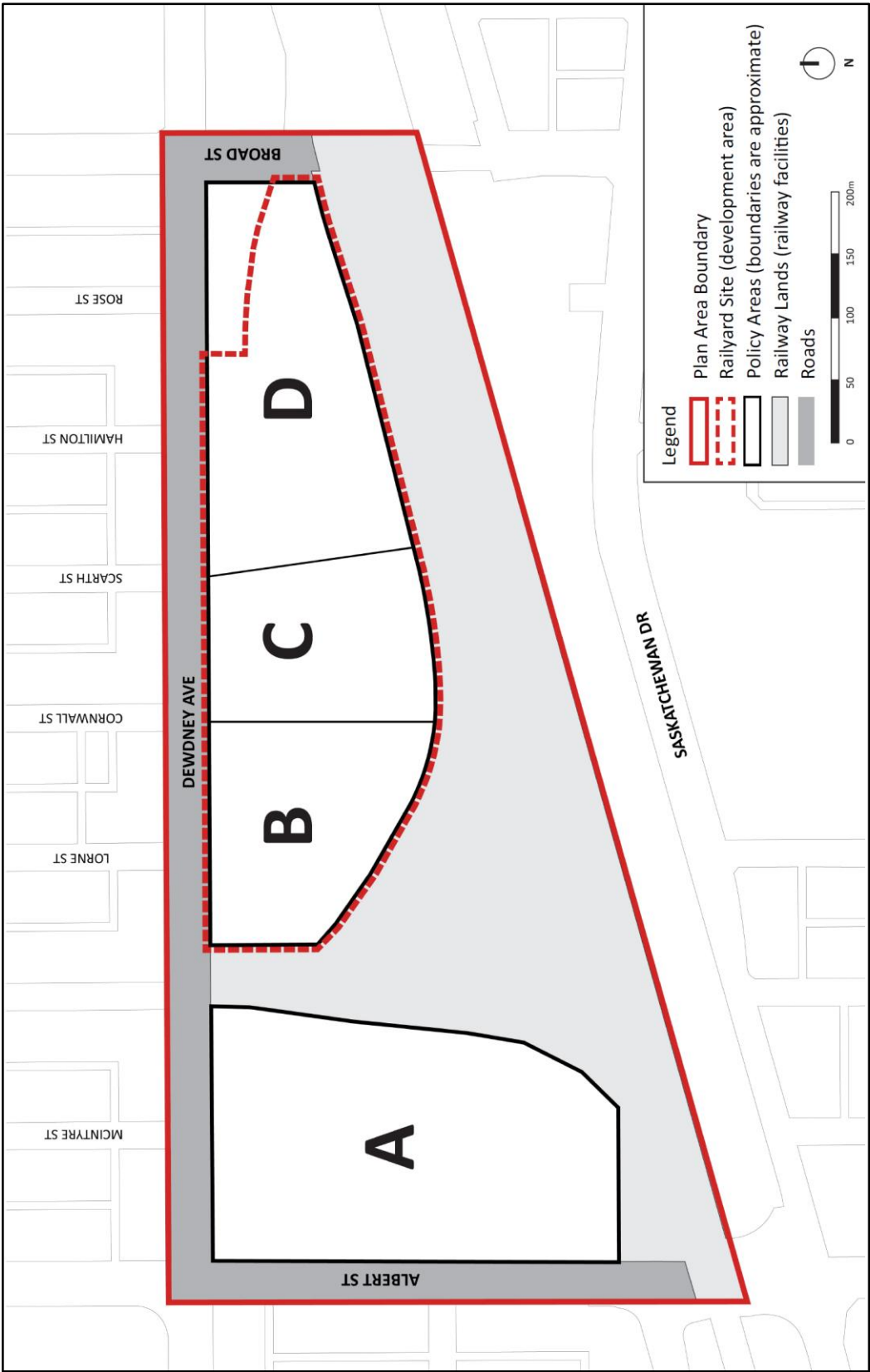
### **Residential**

- f) The City shall endeavour to accommodate the residential objectives of this Plan, as described in Section 4.1, by:
  - i. Supporting and encouraging a mix of appropriate residential types (i.e. multi-unit/apartment; townhouse) throughout the Plan Area, except where policy allows or requires other land-uses in specified areas.
  - ii. Requiring multi-story buildings in specified areas.
- g) The predominant residential form shall be multi-story (vertical multi-unit) buildings, including mixed-use and single-use (residential only) buildings.

### **Railway Proximity**

- h) Residential, institutional, office and commercial buildings shall be set back from the railway corridors in accordance with the following requirements:
  - i. Buildings should be set back at least 30 metres from the property line of the east-west oriented railway corridor, and any associated facilities.
  - ii. Buildings should be set back at least 15 metres from the property line of the north-south oriented railway corridor; however, the City may consider a lesser distance where it can be demonstrated that an appropriate setback from the railway track can be accommodated.
- i) Approval of residential, institutional, office or commercial development immediately adjacent (abutting directly, or abutting an intervening street, lane, buffer, etc.) to the Railway Lands, as shown on Figure 2, shall be contingent on demonstrating that noise and vibration associated with railway activity:
  - i. Is sufficiently low as not to warrant mitigation, or
  - ii. Will be sufficiently mitigated through the incorporation of appropriate building or site features (e.g. landscaped buffer/ berm, parkade, etc.).

Figure 2 – Land Use



## **5. BUILT FORM**

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### **5.1. Overview**

The intent of this section is to provide policy direction for the design of buildings and the relationship between buildings and the public realm. In accordance with the vision and principles of this Plan, the policies herein support a high standard of architectural treatment and unique urban design. Ensuring that buildings support an active public realm, and ensuring appropriate transitions between building heights and massing, will be important objectives of this Plan.

A primary objective is to ensure that the built form transitions from the Downtown to the Warehouse District and that appropriate interface design is established. As such, this Plan requires that taller buildings be located along the south side of the Plan Area and adjacent to the pedestrian bridge and park. Ensuring that Dewdney Avenue is integrated within the open space network, and interfaces appropriately with the Warehouse District to support new economic activity, is another objective. New buildings along Dewdney Avenue will be intimately connected to the street and will be complementary to the unique character of the area.

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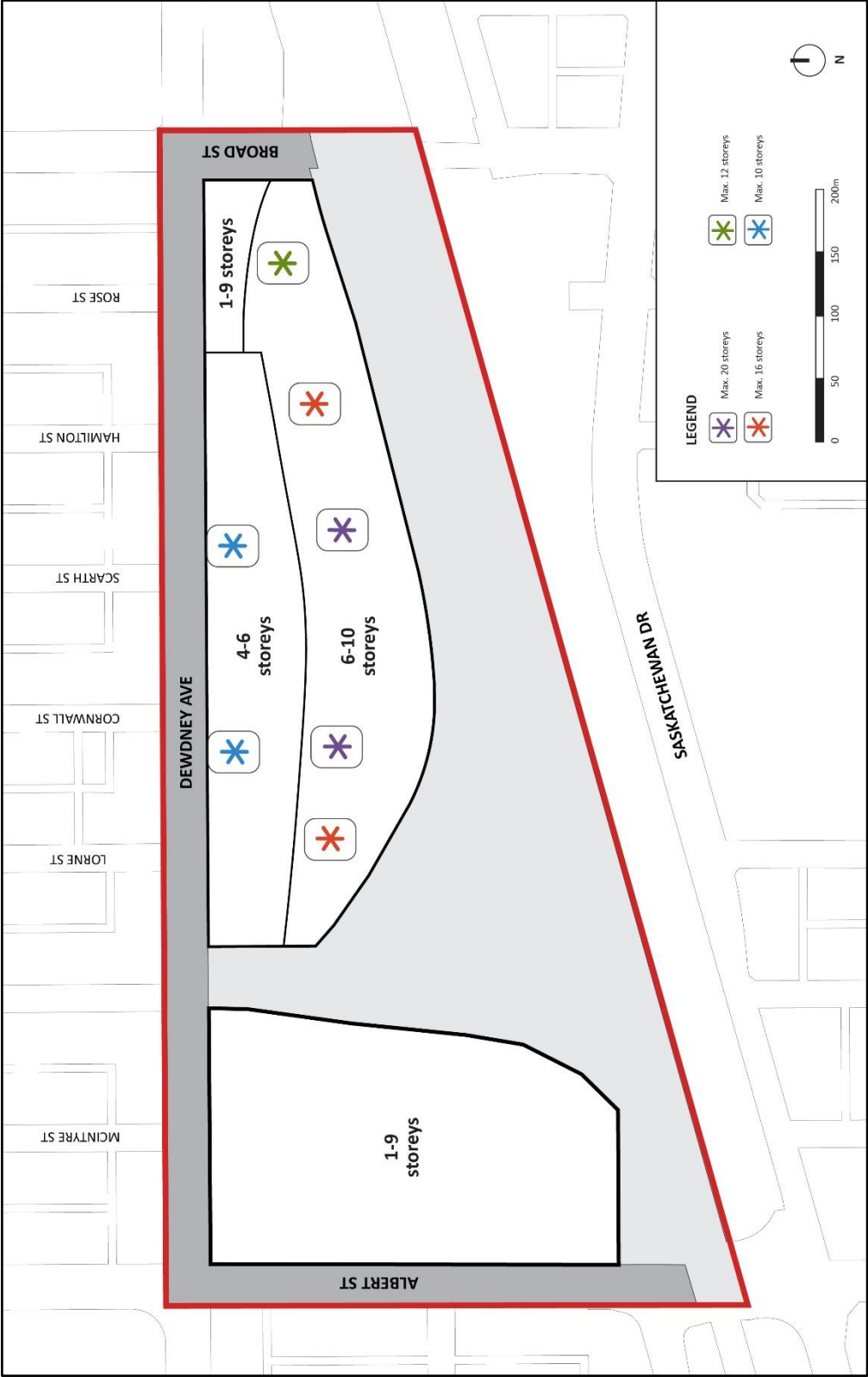
### **5.2. Policy**

- a) The height of buildings should be in accordance with Figure 3.
- b) Notwithstanding Policy 5.2(a) the location of individual buildings, as shown on Figure 3 (indicated with asterisk symbol), shall be considered approximate.
- c) The design of new buildings fronting Dewdney Avenue, within Policy Areas B, C and D, as shown on Figure 2, shall generally complement the design character associated with the historic buildings in Warehouse District, on the opposite side of Dewdney Avenue, by conforming to the following minimum principles:
  - i. The optimal exterior building material, especially for the first three floors, should complement traditional Warehouse District material, including: brick, stone, cast/ engineered stone, etc.
  - ii. Building fronts should reflect a tripartite composition, including base, middle and top, and have well articulated entranceways, fenestration and rooflines.
  - iii. Buildings should follow a minimal front setback and a consistent build-to line.
  - iv. Form and massing and placement of windows, etc., should generally complement traditional Warehouse District style.
- d) The building design principles outlined in Policy 5.2(c) shall be implemented through an amendment to the Zoning Bylaw, which:
  - i. May take the form of a Direct Control District or Architectural Overlay Zone.
  - ii. May include additional or more specific design requirements, beyond what is outlined Policy 5.2(c).

- iii. Includes non-binding urban and sustainable design guidelines, submitted in support of the Zoning Bylaw amendment application, but not forming part of the Zoning Bylaw.
- e) The following types of development shall be exempt from the design requirements stipulated in Policies 5.2(a-c):
  - i. The land-uses supported by Policy 4.2(c).
  - ii. Civic and institutional uses.



Figure 3 – Built Form



## **6. OPEN SPACE**

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### **6.1. Overview**

The intent of this section is to provide policy direction for the implementation of open space features within the Plan Area. Open space features will include a centrally located neighbourhood park and a landscaped buffer that separates new development from the existing railway corridor and that forms part of the open space network and also serves as an amenity feature. Pending further planning and analysis, the open space system may also include pocket parks in strategic locations. Additionally, Dewdney Avenue, as a landscaped boulevard, will complement, and form part of, the open space system by including wide sidewalks, the potential for a multi-use pathway and/ or bike path and landscaped boulevards.

The defining feature of the open space system and the primary location for outdoor recreation, leisure and civic interaction will be a centrally located signature neighbourhood park. The specific design and function of this park will be identified through further planning and analysis and will respond to the specific land-use context that emerges. Major design considerations will include direct views from Dewdney Avenue to the pedestrian bridge; the potential for fronting buildings with active use interface; public art/ cultural features. This park may be phased-in over-time and may include interim use and landscaping.

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### **6.2. Policy**

- a) As shown on Figure 5, open space shall consist, primarily, of:
  - i. A centrally located neighbourhood park (Neighbourhood Park), which may be developed as a landscaped green space, or as a plaza, or as a combination thereof.
  - ii. A landscaped buffer (Landscaped Buffer), which separates the railway corridors from the Plan Area development.
- b) Notwithstanding Policy 6.2(a), through the concept plan process, and without an amendment to this Plan being required, the City may consider:
  - i. Additional parks (e.g. pocket parks).
  - ii. An alternate location and configuration for the proposed Neighbourhood Park, with the proviso that the location is still relatively centralized.
- c) As a prerequisite for concept plan approval, where a park is proposed, or for developing a park, other than an interim park, the City may require that a report be submitted demonstrating the following:
  - i. The proposed programming and design solution.
  - ii. The need and benefit of the proposed park.
  - iii. Conformity with applicable policy and standards.
  - iv. Operation and maintenance implications.
  - v. Construction phasing plan (if applicable).

- d) Open Space intended to accommodate the purposes outlined in Policies 6.2 (a-b) shall:
  - i. Be provided through the retention and utilization of requisite existing City owned lands (irrespective of whether Municipal Reserve designation applies).

#### **Neighbourhood Park**

- e) When locating and designing the Neighbourhood Park, the following design elements shall be taken into consideration:
  - i. Maintaining a view plane, and allowing for a pedestrian and cycling connection, from Dewdney Avenue to the Pedestrian Bridge via the Neighbourhood Park.
  - ii. The potential to accommodate programmed activities, civic gatherings, public art and heritage features.
  - iii. The potential to interface park flanks with active building frontage.

#### **Landscaped Buffer**

- f) A landscaped buffer shall be constructed, which is in general conformity with Figure 4 and Figure 5, and that is supported by a noise and vibration study at the time of detail design without an amendment to this Plan.
- g) Notwithstanding Policy 6.2(f) and Figure 4:
  - i. The City may require an alternate configuration and design concept for the Landscaped Buffer, including more robust noise attenuation, without an amendment to this Plan being required.
  - ii. The Landscaped Buffer may be constructed incrementally over-time.
  - iii. The “Potential Landscaped Buffer”, as shown on Figure 5, shall only be required if residential is proposed adjacent to the corresponding segments of railway corridor and it is demonstrated that a buffer is required for safety or noise mitigation.

**Figure 4 – Landscaped Buffer Design (conceptual only)**

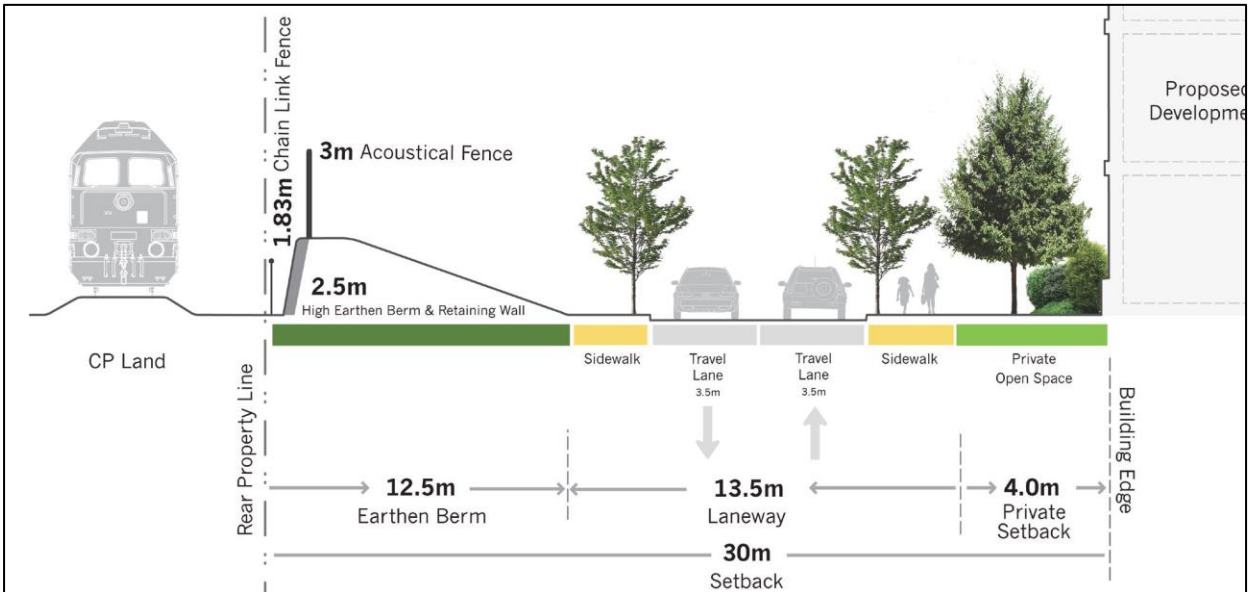
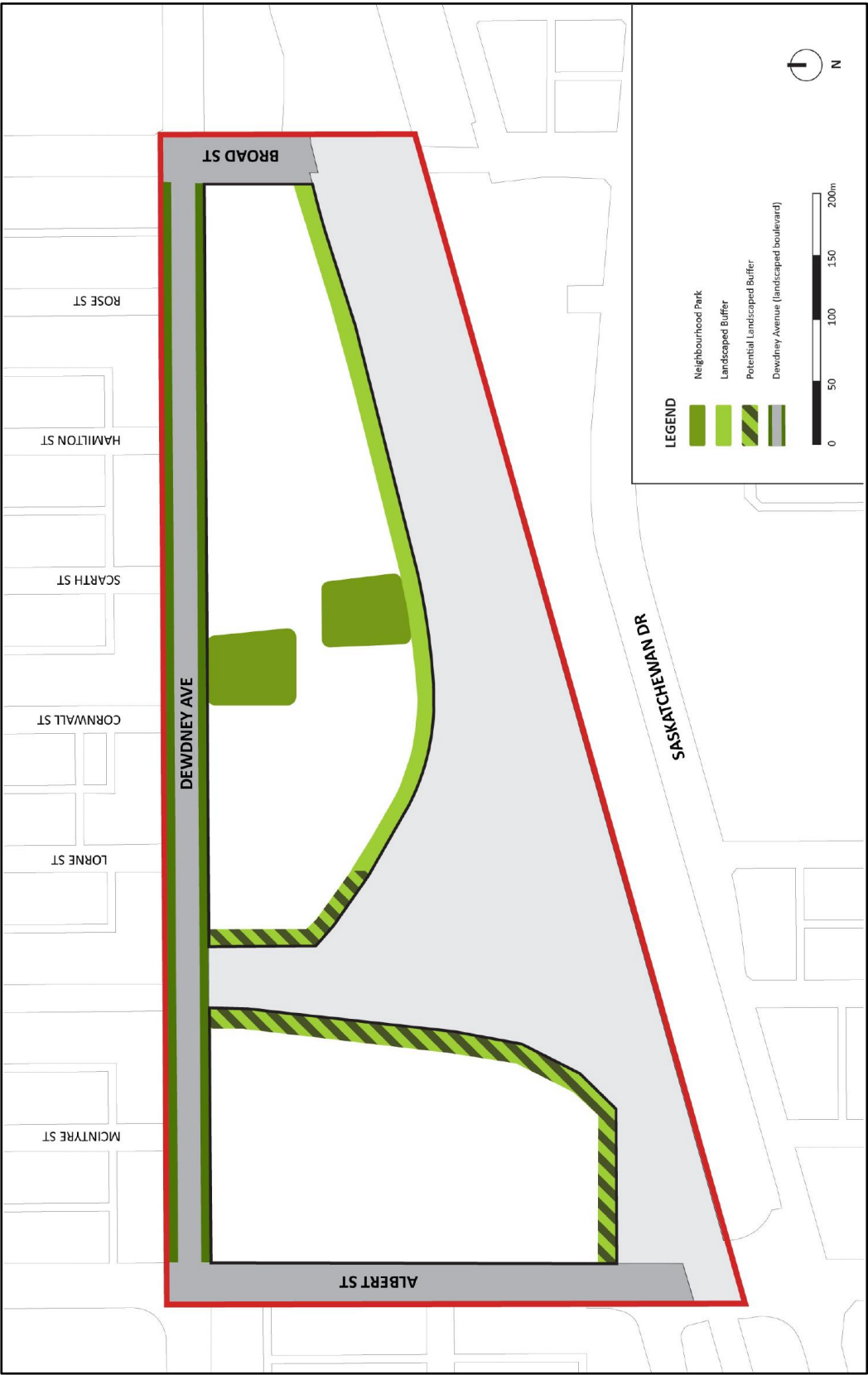


Figure 5 – Open Space



## **7. MOBILITY**

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### **7.1. Overview**

The intent of this section is to provide policy direction for the implementation of the transportation system within the Plan Area. In accordance with the vision and principles of this Plan, the policies support a high level of interconnectivity and an enhanced public realm. Major components of the system will include Dewdney Avenue, as a landscaped boulevard; a pedestrian bridge linking the Downtown with the Plan Area; enhanced Albert Street and Broad Street underpasses; an internal street network. This Plan provides high-level direction for the internal streets only, as the detailed solution will be identified through an approved concept plan.

Dewdney Avenue will form a core component of the transportation system and public realm. The intent is to transition Dewdney Avenue from its current state to a multi-modal landscaped boulevard that accommodates pedestrians, cyclists, transit and vehicles. As a core component of the public realm, Dewdney Avenue will include wide sidewalks, landscaping and furnishings. A pedestrian bridge, crossing the railway corridor, will also form a core component of the transportation system and public realm, linking the Downtown to the Plan Area.

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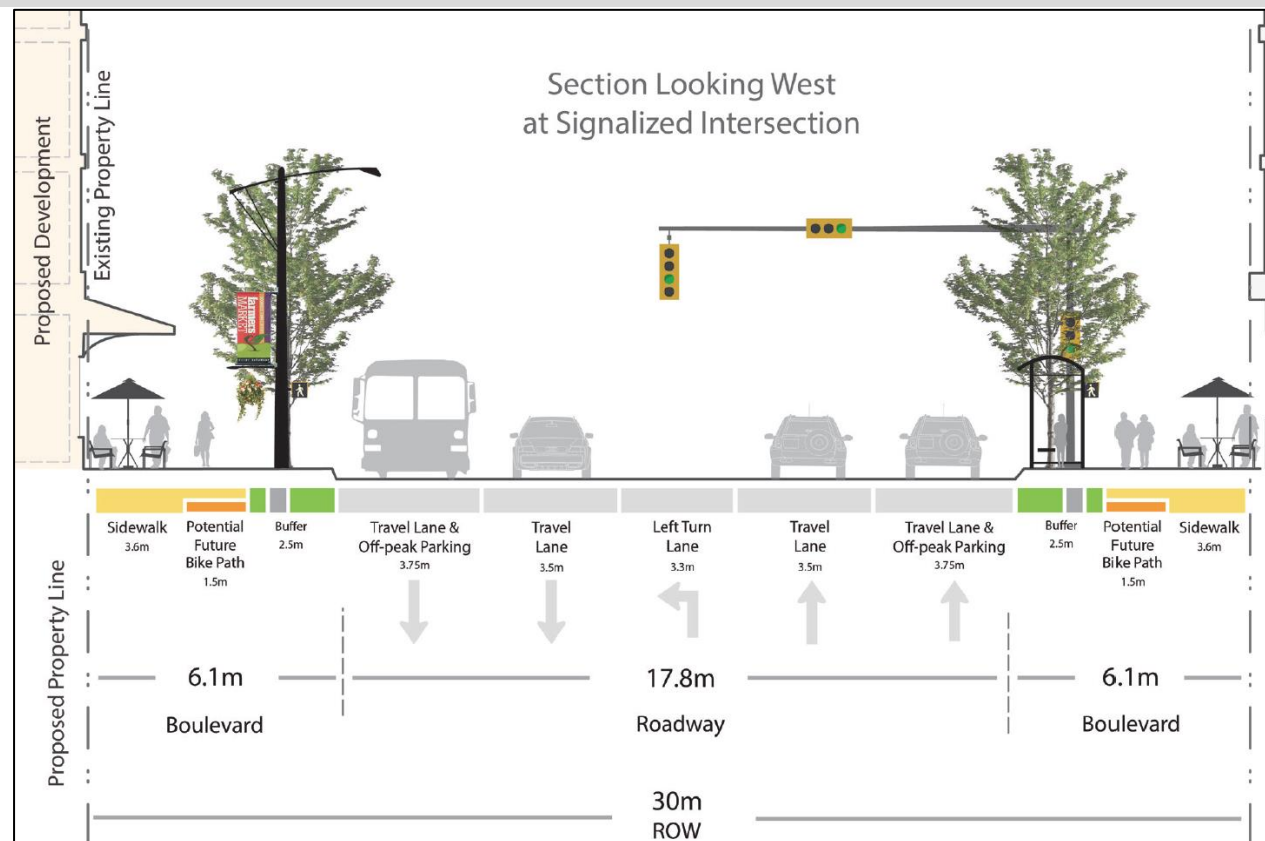
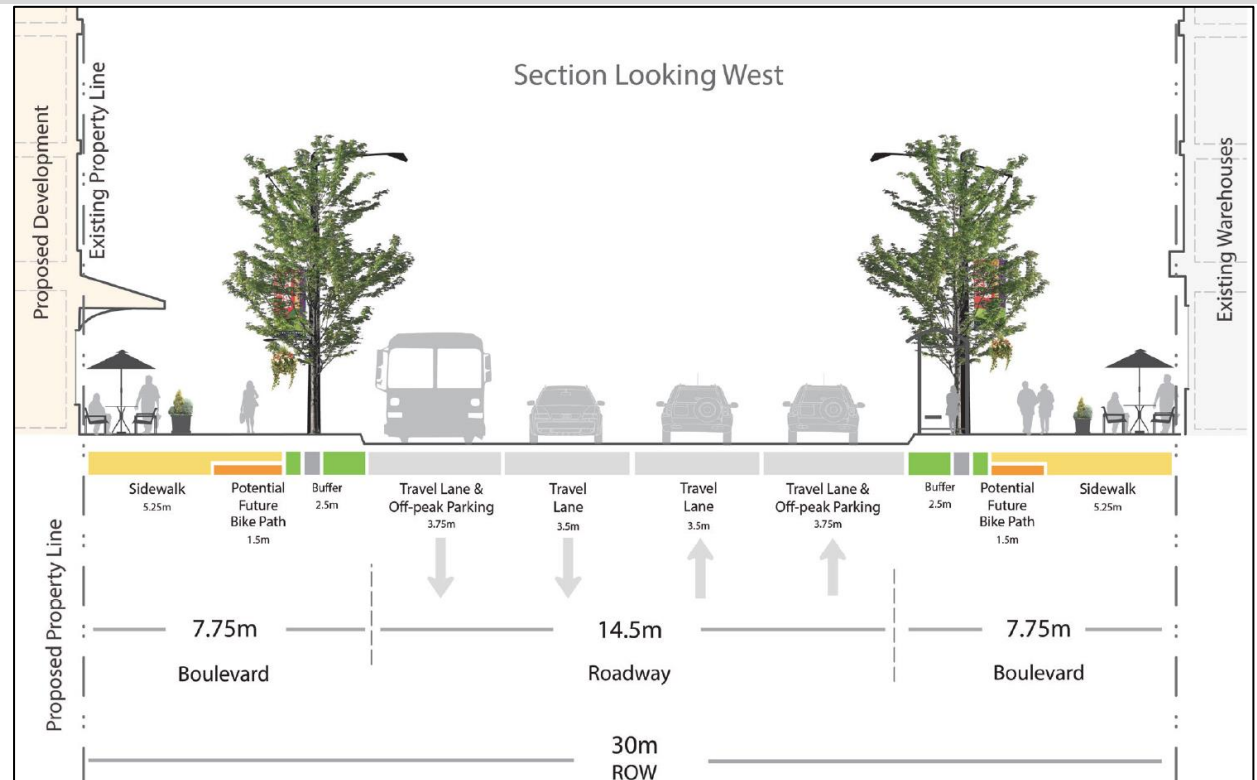
### **7.2. Policy**

- a) The location of transportation infrastructure and the alignment of streets should be in general conformity with Figure 7; however, concept plans and plans of subdivision may accommodate variations without an amendment to this Plan being required.
- b) Streets and lanes within the Plan Area shall support convenient mobility and wayfinding through an interconnected configuration and multiple access points to Dewdney Avenue and, where appropriate, a fine grain block pattern.

#### **Dewdney Avenue**

- c) Dewdney Avenue shall be designed as a landscaped boulevard and:
  - i. Shall include wide sidewalks, suitable for pedestrian strolling and sidewalk shopping.
  - ii. May include a multi-use pathway or bike path.
  - iii. Should be in general conformity with Figure 6; however, the City may consider variations to the concept shown without an amendment to this Plan.

**Figure 6 – Dewdney Avenue Design (conceptual only)**



### **Pedestrian Bridge**

- d) A pedestrian bridge connecting the Plan Area to lands south of the railway corridor (Regina Downtown) should be constructed, which:
  - i. Is located approximately as shown on Figure 7.
  - ii. Connects, on both ends, to public or quasi-public spaces.
  - iii. Represents a safe, active and inviting environment, in all seasons, that is also fully accessible.
  - iv. Reflects a high-level of urban design aesthetics.

### **Railway Underpasses**

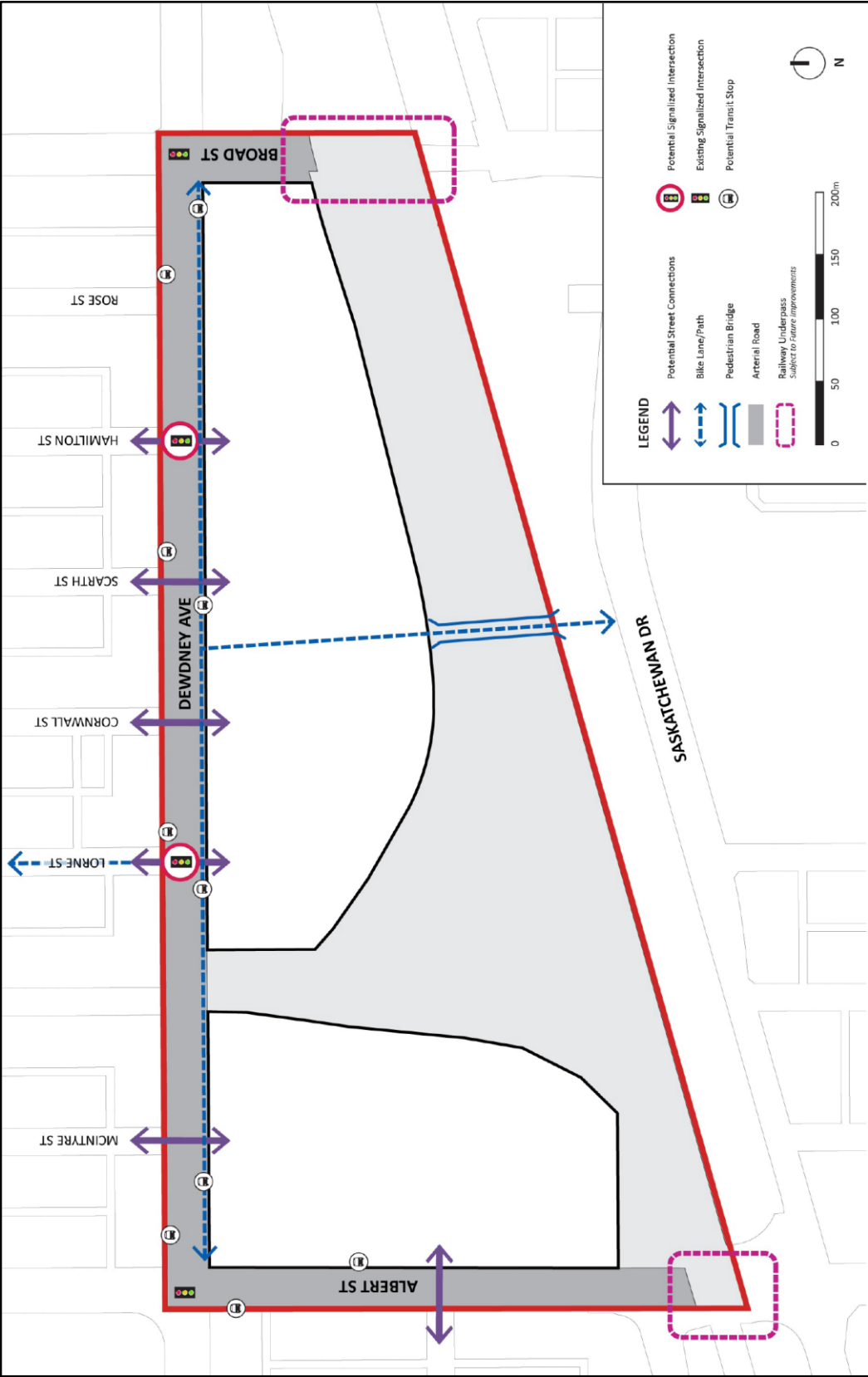
- e) The City shall undertake upgrades to the railway underpasses at Albert Street and Broad Street in order to improve the pedestrian experience, which should include general refurbishment and repair; enhanced lighting; wall/ mural art and, where appropriate, improvements to sidewalks.

### **Parking and Loading**

- f) Loading, servicing and surface parking areas shall be located to the rear or side of associated building and screened from view from the perspective of fronting street, with the exception of:
  - i. Special needs/ accessible parking.
  - ii. Parking that is main/ principal use of a parcel.
- g) Parking for buildings should primarily be accommodated underground or, where not feasible, be located to the rear or side of buildings.
- h) Above-ground parking structures (garages) shall not be permitted to front, directly, Dewdney Avenue, Albert Street or Broad Street.



Figure 7 – Mobility Plan



## **8. Servicing**

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### **8.1. Overview**

The purpose of this section is to provide policy direction for the implementation of utility services (e.g. water, wastewater, stormwater) within the Plan Area. The servicing strategy for this Plan focuses on the Railyard Site, as the other lands within the Plan Area are either substantially developed or form part of the ongoing railway facilities. Further, the figures of this Plan illustrate the major servicing connection points only, as the detailed, internal, networks will be identified through the concept plan process. A summary of the servicing strategy follows:

#### Water Servicing

- The Railyard Site is proposed to include two tie-ins to an existing 600mm watermain within Dewdney Avenue, at the intersection of Dewdney Avenue and Lorne Street and the intersection of Dewdney Avenue and Hamilton Street.
- Upgrades to the city-wide water system, will provide service to the new development.

#### Wastewater Servicing

- To service the Railyard Site, a connection to the existing 375mm sanitary sewer main at the intersection of Hamilton Street and Dewdney Avenue is proposed.
- Existing topography, as well as the existing invert elevations at the tie-in point of the existing 375 sanitary sewer main, suggests the Railyard Site to be serviced via gravity flow; there is not expected to be a need for an internal lift station.
- Upgrades to the city-wide wastewater system, will provide service to the new development.

#### Stormwater Servicing

- An internal underground network that ties into an oversized in-line detention storage is proposed to service the Railyard Site, for both design storms (1 in 5 and 1 in 25 year).
- The system will discharge at a restricted release rate to existing underground detention storage for the existing Broad Street Lift Station.
- Existing topography, as well as the existing invert elevation at the tie-in point of the existing storm main, allow the entire Railyard Site to be serviced via gravity flow; there is no need for an internal lift station.

## **8.2. Policy**

- a) Major water, wastewater and stormwater servicing should be in general conformity with Figures 8-10 and as described in Section 8.1; however, the City may permit an alternate servicing plan without an amendment to this Plan being required.
- b) Notwithstanding Policy 8.2(a), water, wastewater and stormwater servicing, including networks internal to the Railyard Site, shall be in general conformity with servicing reports prepared to the City's satisfaction, and submitted to the City through the concept plan process or, at the City's discretion, rezoning or subdivision.
- c) At the time of concept plan approval, a water servicing report shall be prepared for the Railyard Site, prior to approval of the concept plan.
- d) At the time of concept plan approval, a wastewater servicing report shall be prepared for the Railyard Site, prior to approval of the concept plan and shall conform to the Wastewater Master Plan that is in effect.

Figure 8 – Water Servicing

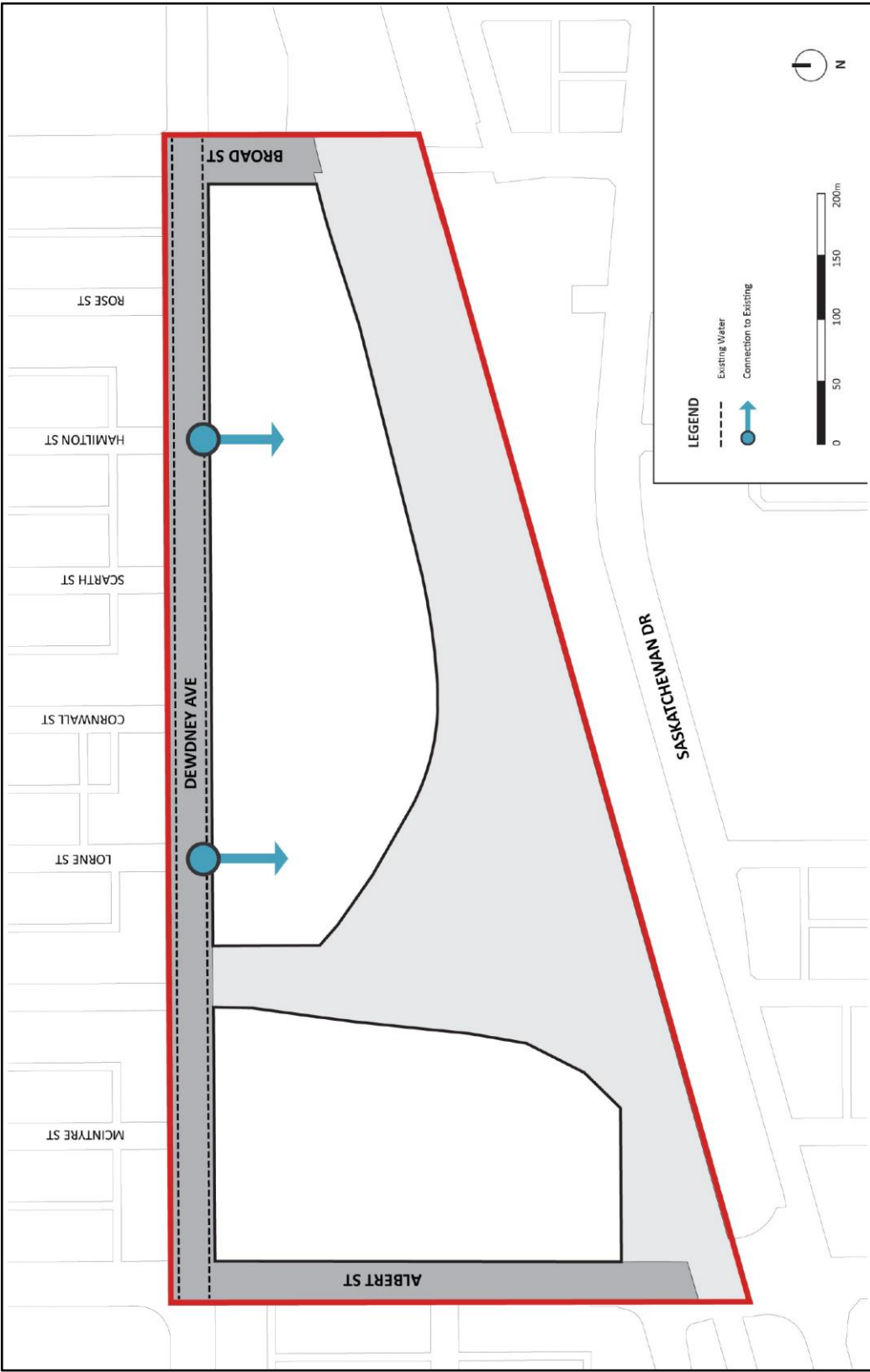


Figure 9 – Wastewater Servicing

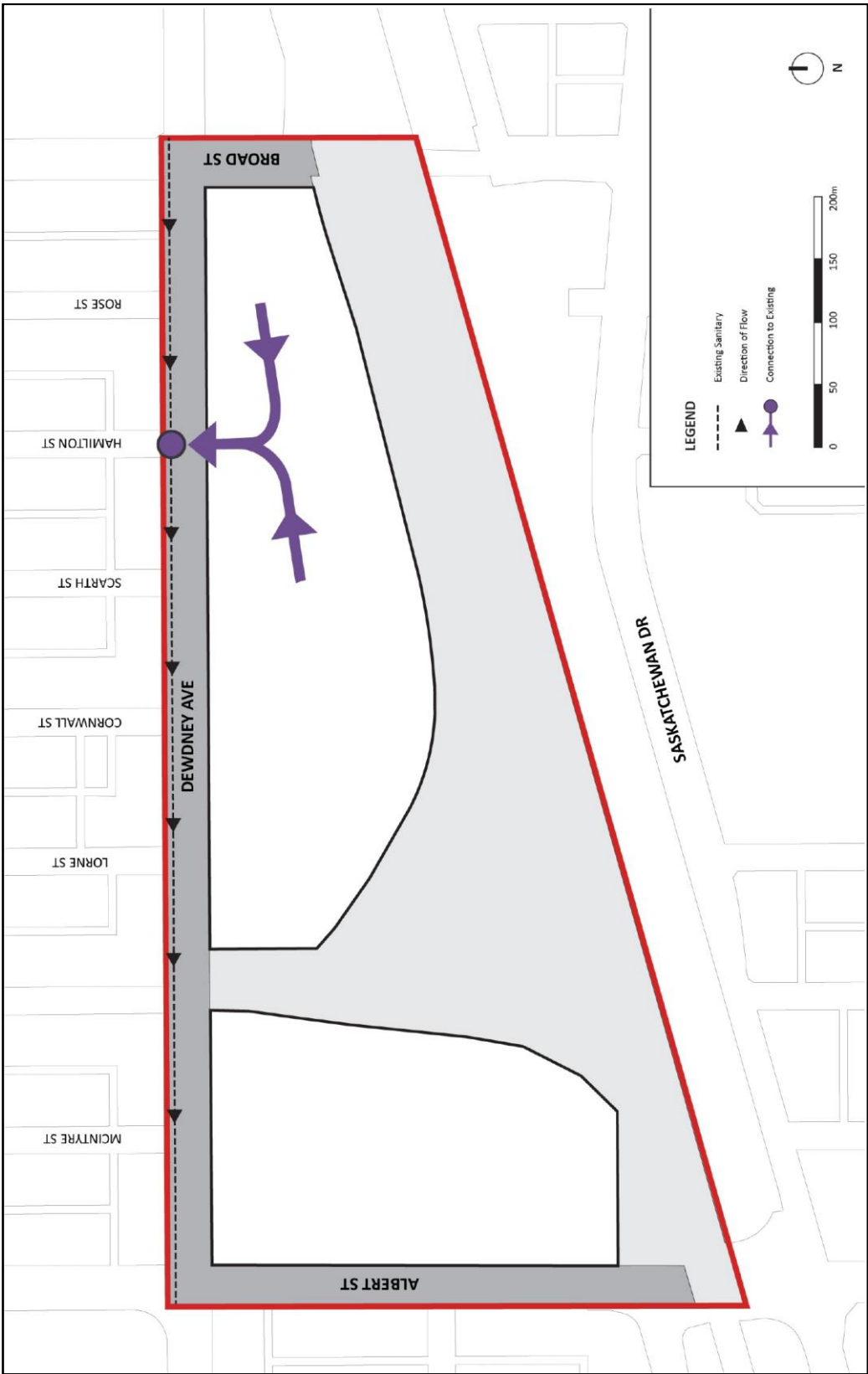
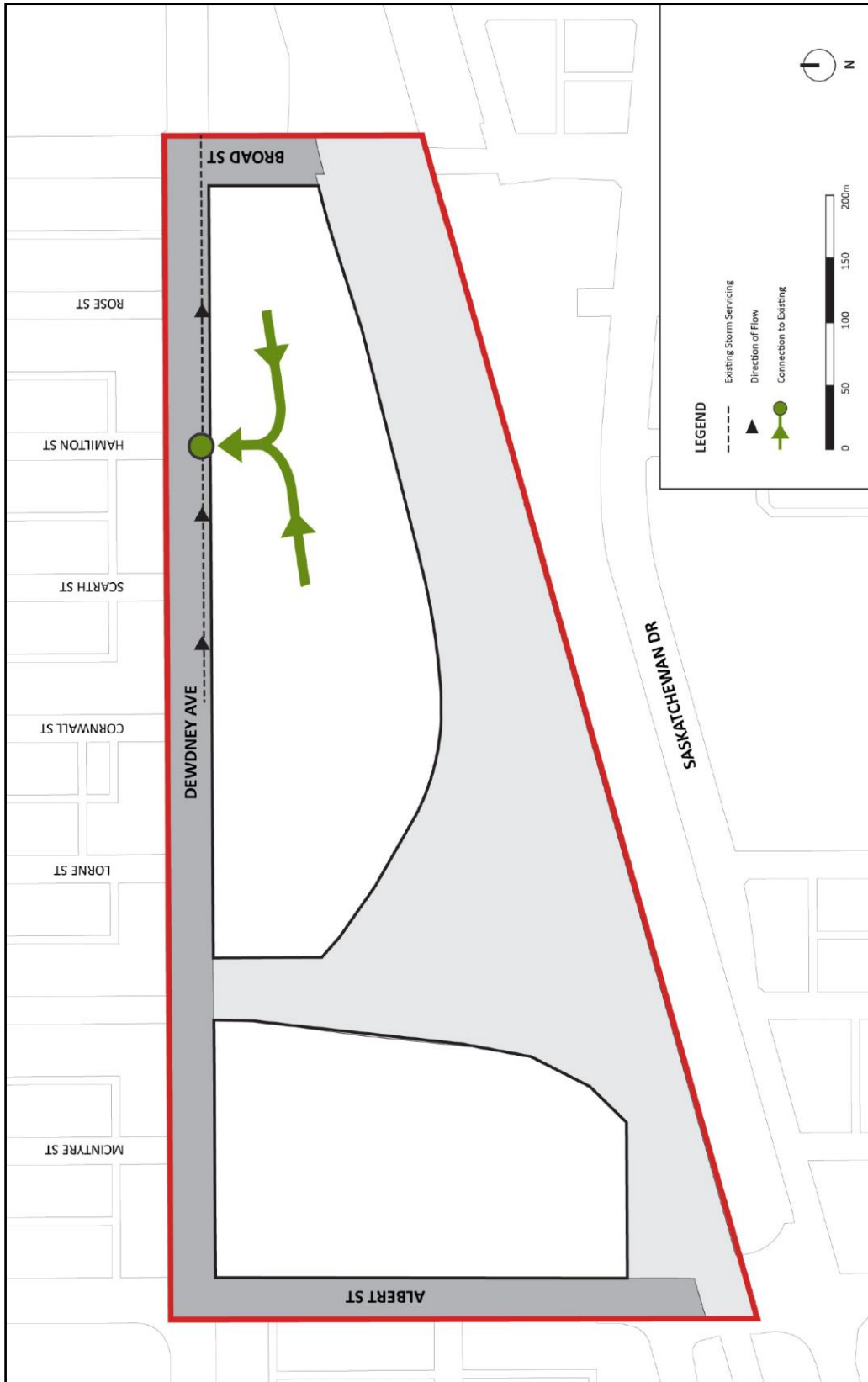


Figure 10 – Stormwater Servicing



## 9. IMPLEMENTATION

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### 9.1. Overview

This Plan provides policy direction for the growth, development and servicing of the Plan Area, as well as direction for more detailed planning and engineering work that will occur overtime. Prior to development occurring, a concept plan must be prepared, followed by rezoning. The concept plan and zoning designation must be in conformity with this Plan but should respond to a defined and articulated development concept. Site improvements required for development include remediation of impacted areas and the construction of utility infrastructure. Additionally, the City will be pursuing other upgrades, as noted below, which will enhance the site. This inventory is not exhaustive and the prioritization schedule is subject to change.

Project	Priority
• Dewdney Avenue Upgrade (1 <sup>st</sup> Phase)	Initial
• Landscaped Buffer	Initial
• Albert Street and Broad Street Underpasses	Secondary
• Neighbourhood Park (Interim)	Secondary
• Dewdney Avenue Upgrade (2 <sup>nd</sup> Phase)	Secondary
• Internal Street/ Utilities	As Per Build-Out
• Neighbourhood Park (Ultimate)	As Per Build-Out
• Pedestrian Bridge	Future

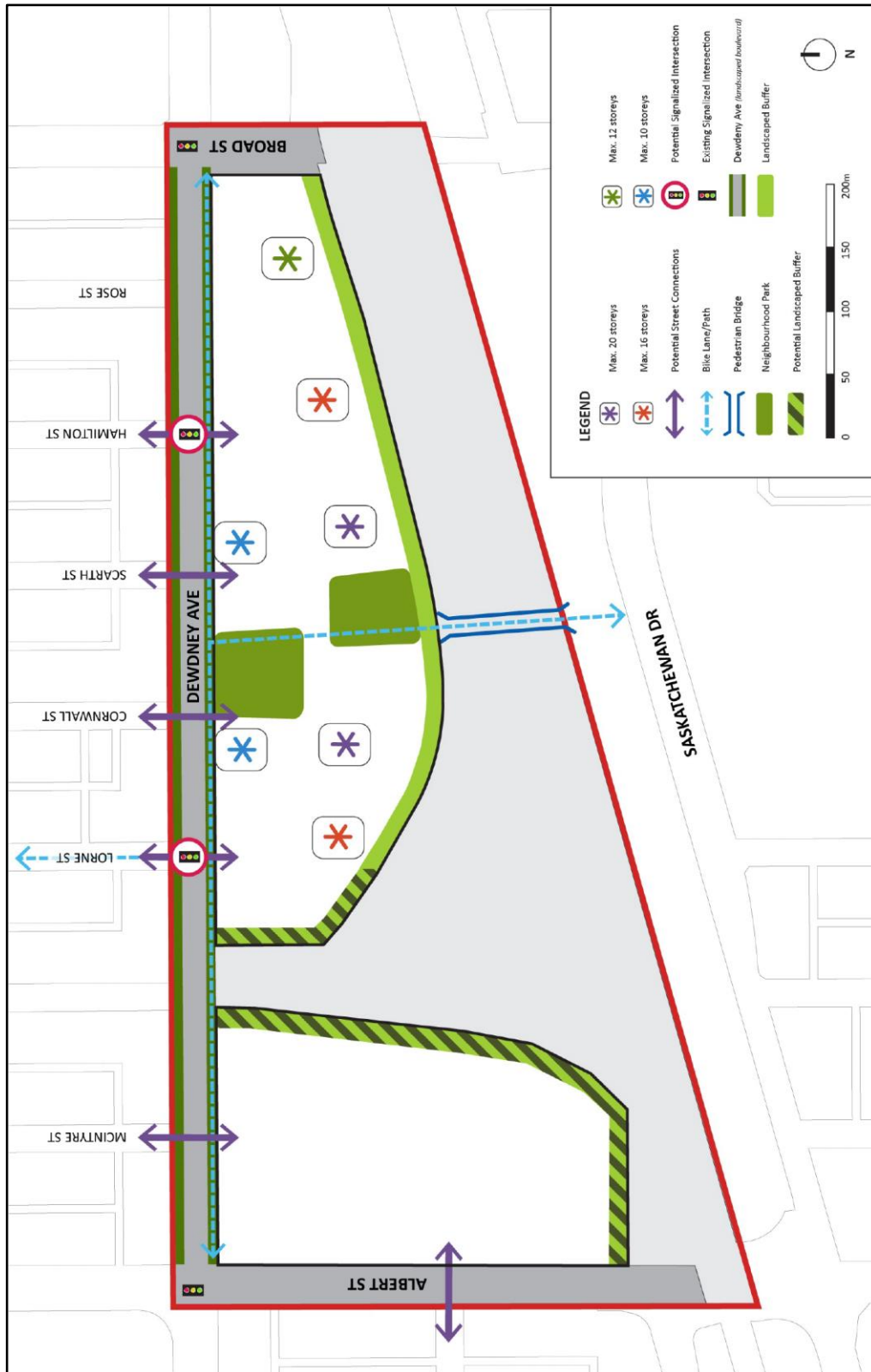
### 9.2. Policy

- a) As a prerequisite for rezoning, subdivision or development approval, within Mixed-Use Areas B, C and D, as shown on Figure 2, the City may require that:
  - i. Any further, or more detailed, environmental site analysis or geotechnical analysis and/or Quantitative Risk Assessment be undertaken and submitted to the City and/ or Government of Saskatchewan.
  - ii. Any site remediation required by the City and/ or Government of Saskatchewan be undertaken.
  - iii. It be demonstrated that the site is suitable for the intended use, from the perspective of geophysical, environmental and land-use compatibility conditions, or can be made suitable through particular measures.
  - iv. It be demonstrated that an adequate level of utility and transportation services can be provided without significantly diminishing servicing levels elsewhere.
- b) As a prerequisite for rezoning or subdivision approval, within the Railyard Site area, as shown on Figure 2, a concept plan must be approved.
- c) An amendment to the Zoning Bylaw may be required to ensure that development is in accordance with the Vision, Principles and Policies of this Plan; however, the

specifications of this amendment (e.g. applicable regulations, land area, timing) may be deferred until a proposed development/ land-use application warrants the amendment.



## Appendix A – Master Plan



## Review of Outstanding Items

<b>Date</b>	January 8, 2020
<b>To</b>	Regina Planning Commission
<b>From</b>	City Clerk's Office
<b>Service Area</b>	Office of the City Clerk
<b>Item No.</b>	RPC20-4

## RECOMMENDATION

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It is recommended that Regina Planning Commission:

1. Delete the following items from its List of Outstanding Items:

<u>Item</u>	<u>Committee</u>	<u>Subject</u>
RPC04-16	Regina Planning Commission	Regina's Old Warehouse Business Improvement District: Warehouse District Planning Study
MN11-10	Regina Planning Commission	Zoning Bylaw - Contractor Yards in Residential Areas
CR18-116	Regina Planning Commission	Civic Naming Committee Guideline Review
RPC12-17	Regina Planning Commission	Rezoning and Discretionary use Application (12-Z-20/12-DU-24) - Proposed Fourplex - 4000 3rd Avenue, Windsor Place Subdivision
CR14-137	Regina Planning Commission	Lease of Road Right-of-Way

2. Forward the updated List of Outstanding Items to Executive Committee for information.

## ISSUE

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Subsection 35(2) of City Council's Procedure Bylaw requires the City Clerk to provide an annual report to the Executive Committee which lists all items and the priority of the items that have been tabled or referred by City Council or one of its committees. The purpose of this report is to provide a list of the outstanding items for Regina Planning Commission as at December 31, 2019.

## **IMPACTS**

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Regular review of outstanding items provides both Council and the City Administration an opportunity to review and refocus priorities and resources as required based on current initiatives, needs of the community and corporate strategy.

## **OTHER OPTIONS**

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None.

## **COMMUNICATIONS**

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No specific public communication is required, however, this report will be posted to the City of Regina website for public viewing.

## **DISCUSSION**

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Lists of Outstanding Items are maintained for City Council and its main committees. Items on the list may originate from:

- a recommendation in a report which indicates that another report will be forthcoming;
- a motion adopted to refer an item back to the Administration or to request a report on a related matter;
- a motion adopted by City Council or another committee requesting the Administration to prepare a report.

The Office of the City Clerk is responsible for maintaining and updating the lists and items remain on the list until a report or the committee recommends their removal. Lists are updated with additions and deletions, as meetings are held.

The outstanding items report is first circulated to Regina Planning Commission to have a detailed discussion of each item with Administration and among members to determine priorities for Council consideration. The updated list is then sent to Executive Committee for further consideration and direction to the City Manager for any changes in priority.

## DECISION HISTORY

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The last review of outstanding items as at November 30, 2018 was considered by Executive Committee on February 13, 2019.

Respectfully submitted,



Amber Ackerman,  
Deputy City Clerk

Respectfully submitted,



Jim Nicol,  
City Clerk

Prepared by: Elaine Gohlke, Council Officer

## ATTACHMENTS

Appendix A

**Regina Planning Commission  
List of Outstanding Items  
December 31, 2019**

REPORT #:	RPC04-16
DATE TABLED/REFERRED:	3/24/2004
SUBJECT:	Regina's Old Warehouse Business Improvement District: Warehouse District Planning Study
MOTION:	This communication be referred to the Administration for review and analysis with reports to the various standing committees within six months on the implications of implementing the various components of the Warehouse District Planning Study.
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: <b>Addressed by PPC19-2 Neighbourhood and Corridor Sequencing Plan at the February 20 PPC meeting. Remove from list at end of 2019.</b>

REPORT #:	RPC10-5
DATE TABLED/REFERRED:	2/24/2010
SUBJECT:	Cell Phone Towers
MOTION:	This communication be referred to the Administration for a report on guidelines and/or principles for cell phone towers on City of Regina property.
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: Cell towers are within federal jurisdiction, but the federal approval process allows municipalities to file a protocol document outlining expectations on community consultation and a process for selecting tower locations that providers must follow. A report to City Council to determine the level of City involvement in cell tower development is planned for Q4 2020.

**Regina Planning Commission  
List of Outstanding Items  
December 31, 2019**

REPORT #:	MN11-10
DATE TABLED/REFERRED:	9/19/2011
SUBJECT:	Zoning Bylaw - Contractor Yards in Residential Areas
MOTION:	<ol style="list-style-type: none"><li>1. That City Council instruct the Administration to review the Zoning Bylaw in relation to Contractor Yards, including parking, with a view to clarifying or establishing wording in the Bylaw that clearly identifies what is permitted in residential areas including equipment storage.</li><li>2. That the Administration be instructed to review the Land Use Development Regulations Chart to ensure it clearly identifies for the public what is and is not permissible in each zoned area.</li></ol>
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: <b>Addressed through Zone Forward. Remove from list at end of 2019.</b>

REPORT #:	MN12-1
DATE TABLED/REFERRED:	1/23/2012
SUBJECT:	Sustainable Commercial and Industrial Buildings Incentive Program
MOTION:	<p>That City Council instruct the Administration to prepare a report, as part of the Design Regina process, which:</p> <ol style="list-style-type: none"><li>1. considers emerging best practices</li><li>2. Incorporates any relevant legal considerations</li><li>3. Includes stakeholder input; and</li></ol> <p>provides recommendations for how the city could incent or encourage the development community to incorporate green, sustainable best practices in future commercial and industrial construction projects.</p>
DIVISION/DEPARTMENT:	TBD
COMMENT:	Return Date: Pending determination of corporate lead.

**Regina Planning Commission  
List of Outstanding Items  
December 31, 2019**

REPORT #:	RPC12-17
DATE TABLED/REFERRED:	9/13/2012
SUBJECT:	Rezoning and Discretionary use Application (12-Z-20/12-DU-24) - Proposed Fourplex -4000 3rd Avenue, Windsor Place Subdivision
MOTION:	5. That Administration work with the Legal Department to explore options for architectural controls and provide a report to the Regina Planning Commission in the first quarter of 2013
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: <b>Localized discussion about options for architectural control will occur at the community level during the Neighbourhood Plan process. Remove from list.</b>

REPORT #:	CR14-137
DATE TABLED/REFERRED:	11/27/2014
SUBJECT:	Lease of Road Right-of-Way
MOTION:	That the Administration report back to Regina Planning Commission in Q2 of 2015 on the criteria on permanent signs as it relates to aesthetics, revenue and statistics on the number of signs within the city limits.
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: <b>Addressed through Zone Forward. Remove from list at end of 2019.</b>

**Regina Planning Commission  
List of Outstanding Items  
December 31, 2019**

REPORT #:	RPC15-31
DATE TABLED/REFERRED:	6/3/2015
SUBJECT:	Application for Sale of Dedicated Lands (15-SD-01) Portion of Qu'Appelle Park - 1301 Parker Avenue
MOTION:	That Administration conduct a review of the policy related to the sale of parcels of City land for the installation of cell towers, including the size of the parcel and related setbacks, as well as any related Bylaw changes that may be required.
DIVISION/DEPARTMENT:	City Planning & Development
COMMENT:	Return Date: To be included in report to City Council to determine the level of City involvement in cell tower development. Q4 2020.

REPORT #:	CR18-116
DATE TABLED/REFERRED:	11/26/2018
SUBJECT:	Civic Naming Committee Guideline Review
MOTION:	8. That the Administration prepare a report on criteria to apply when writing a report addressing the historical legacy of the namesake by Q2 of 2019 and submit the report for consideration to City Council.
DIVISION/DEPARTMENT:	City Clerk's Office
COMMENT:	Return Date: <b>Addressed by RPC19-27 at the November 6 RPC Meeting. Remove from list at end of 2019.</b> "

REPORT #:	CR19-83
DATE TABLED/REFERRED:	9/30/2019
SUBJECT:	Discretionary Use Application (19-DU-07) Proposed Residential Homestay – 3118 Albert Street
MOTION:	That this matter be referred to Administration for regulations and a licensing policy framework to be brought forward to Regina Planning Commission by Q1 of 2020.
DIVISION/DEPARTMENT:	Planning & Development Services
COMMENT:	Return Date: March 31, 2020